



Gray's Crossing Specific Plan

January 2004

Amended January 9, 2018



PLANNING & ENGINEERING, INC



Gray's Crossing Specific Plan

January 2004

Prepared for:

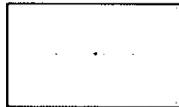


East West Partners, Developer

Gray's Station, LLC, Owner

12257 Business Park Drive, Suite 8
Truckee, CA 96161
(530) 587-3460

Prepared by:



SCO Planning & Engineering, Inc.

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Grass Valley, CA 95945
(530) 272-5841

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HART | HOWERTON

Hart|Howerton

30 Hotaling Place
San Francisco, CA 94111
(415) 986-4260

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Maps

AERIAL PHOTO FOR GRAYS CROSSING



1
7



TOWN OF TRUCKEE

AERIAL PHOTO FOR:
GRAYS CROSSING
PLANNED DEVELOPMENT

CALIFORNIA

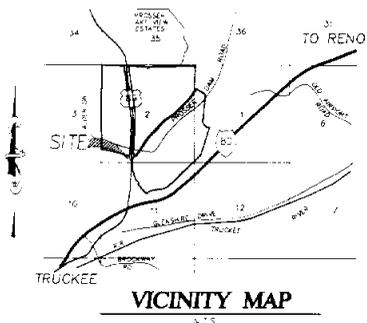
NO.	REVISIONS	DATE	DESIGNER
			DAVID NEW
			ERIC MC GIBBY
			DR. MEL GAY'S A/P
			DR. GEE GUSTAF

OVERALL DEVELOPMENT PLAN

FOR GRAY'S CROSSING

(REVISED 11-26-03)

BEING SECTION 2 AND A PORTION OF THE N 1/4 SECTION 11, TOWNSHIP 17
NORTH, RANGE 16 EAST, M.D.M., IN THE TOWN OF TRUCKEE, CALIFORNIA



PROJECT INFORMATION:

OWNER/APPLICANT:
TRUCKEE AND
A TRUCKEE, NORTH CAROLINA COMPANY
P.O. BOX 2237
TRUCKEE, NC 28786
(813) 587-3490

PLANNING & ENGINEERING:
SCOTT BARNES & ASSOCIATES, INC.
2001 HIGHWAY 7, SUITE 240
GRASS VALLEY, CA 95945
(530) 479-5841
15800 TRUCKEE AVENUE, SUITE 102
TRUCKEE, CA 96203
(530) 479-5841

PLANNING & ARCHITECTURE:
DAVE DAVIS & ASSOCIATES
1919 EAST 10TH STREET, SUITE 404
DENVER, CO 80202
(303) 733-8888
CONTACT PERSON: DAVE DAVIS
ARC 556 1276

ASSessor's PARCELS:
19-040-17 THROUGH 19-040-26 AND 19-040-27
19-040-28 AND 19-040-29 PORTIONS OF
19-040-30, 19-040-31 AND 19-040-32

LAND AREA:
TOTAL AREA: 1012 ACRES

ZONE:
RC-2

GENERAL PLAN DESIGNATION:
RC-2

FIRE PROTECTION:
TRUCKEE FIRE PROTECTION DISTRICT

WATER:
TRUCKEE WATER TREATMENT PLANT DISTRICT

SEWERAGE DISPOSAL:
TRUCKEE SANITARY DISTRICT

ELECTRICAL UTILITIES:
TRUCKEE POWER PLANT, LOT 19-040-01

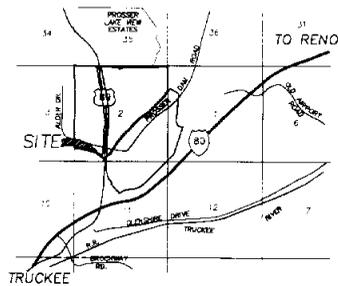
LEGEND:

- WETLANDS (DELINEATED BY ECOSYNTHESIS - OCT. 2001)
- 100 YEAR FLOOD PLAIN
- EXISTING DRAINAGE
- PROPOSED 4' CLASS II TRAIL
- PROPOSED 4' SOFT SURFACE TRAIL
- PROPOSED 8' SEPARATED PAVED SURFACE TRAIL
- PROPOSED 8' CLASS I TRAIL
- APPROXIMATE GOLF CART PATH ROUTING

SINGLE-FAMILY	- 408 LOTS
COTTAGE HOMES	- 89 LOTS
ATTACHED	- 115 UNITS
AFFORDABLE/ EMPLOYEE COMPLEX	- 92 UNITS
VILLAGE AREA	- 21 LOFTS
TOTAL	- 725

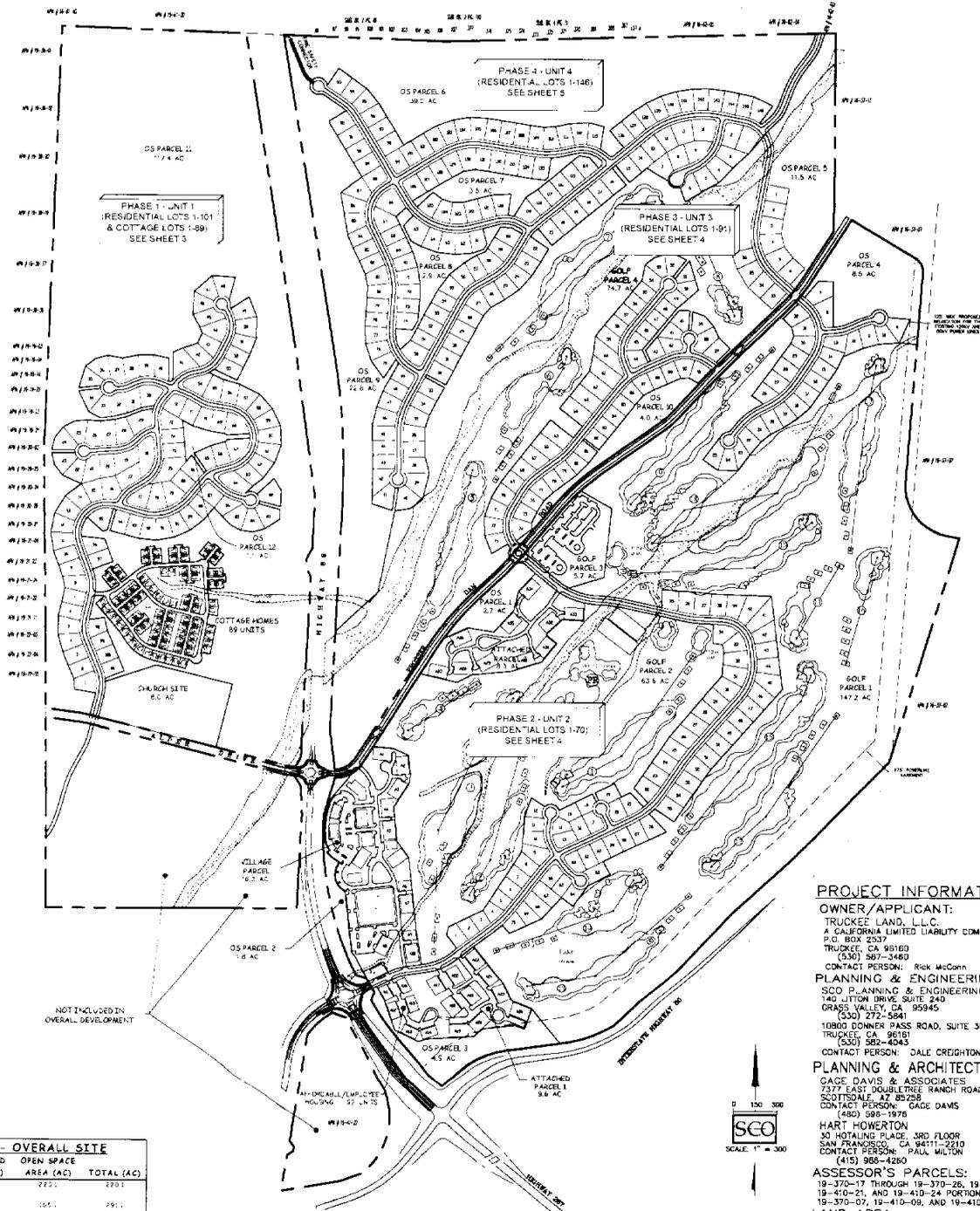
TENTATIVE MAP FOR GRAY'S CROSSING

(REVISED)
BEING SECTION 2 AND A PORTION OF THE N. 1/4 SECTION 14, TOWNSHIP 17 NORTH, RANGE 16 EAST, M.D.M., IN THE TOWN OF TRUCKEE, CALIFORNIA



VICINITY MAP

N.T.S.

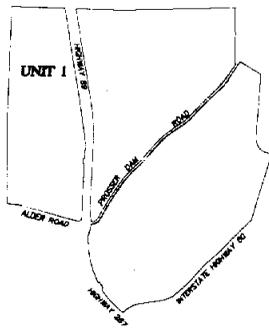


NOT INCLUDED IN OVERALL DEVELOPMENT

USE	DEVELOPED AREA (AC)	OPEN SPACE AREA (AC)	TOTAL (AC)
OPEN SPACE PARCELS		220.1	220.1
OS PARCEL 11		11.4	11.4
OS PARCEL 12		11.4	11.4
OS PARCEL 1		1.6	1.6
OS PARCEL 2		1.6	1.6
OS PARCEL 3		4.5	4.5
OS PARCEL 4		22.8	22.8
OS PARCEL 5		11.5	11.5
OS PARCEL 6		39.2	39.2
OS PARCEL 7		3.5	3.5
OS PARCEL 8		27.5	27.5
OS PARCEL 9		22.8	22.8
OS PARCEL 10		40.0	40.0
OS PARCEL 13		14.7	14.7
OS PARCEL 14		6.5	6.5
OS PARCEL 15		147.2	147.2
OS PARCEL 16		147.2	147.2
OS PARCEL 17		6.7	6.7
OS PARCEL 18		2.7	2.7
OS PARCEL 19		9.1	9.1
OS PARCEL 20		63.5	63.5
OS PARCEL 21		63.5	63.5
OS PARCEL 22		4.5	4.5
OS PARCEL 23		1.6	1.6
OS PARCEL 24		1.6	1.6
OS PARCEL 25		1.6	1.6
OS PARCEL 26		1.6	1.6
OS PARCEL 27		1.6	1.6
OS PARCEL 28		1.6	1.6
OS PARCEL 29		1.6	1.6
OS PARCEL 30		1.6	1.6
OS PARCEL 31		1.6	1.6
OS PARCEL 32		1.6	1.6
OS PARCEL 33		1.6	1.6
OS PARCEL 34		1.6	1.6
OS PARCEL 35		1.6	1.6
OS PARCEL 36		1.6	1.6
OS PARCEL 37		1.6	1.6
OS PARCEL 38		1.6	1.6
OS PARCEL 39		1.6	1.6
OS PARCEL 40		1.6	1.6
OS PARCEL 41		1.6	1.6
OS PARCEL 42		1.6	1.6
OS PARCEL 43		1.6	1.6
OS PARCEL 44		1.6	1.6
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OS PARCEL 48		1.6	1.6
OS PARCEL 49		1.6	1.6
OS PARCEL 50		1.6	1.6
OS PARCEL 51		1.6	1.6
OS PARCEL 52		1.6	1.6
OS PARCEL 53		1.6	1.6
OS PARCEL 54		1.6	1.6
OS PARCEL 55		1.6	1.6
OS PARCEL 56		1.6	1.6
OS PARCEL 57		1.6	1.6
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OS PARCEL 76		1.6	1.6
OS PARCEL 77		1.6	1.6
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OS PARCEL 212		1.6	1.6
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OS PARCEL 217		1.6	1.6
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OS PARCEL 229		1.6	1.6
OS PARCEL 230		1.6	1.6
OS PARCEL 231		1.6	1.6
OS PARCEL 232		1.6	1.6
OS PARCEL 233		1.6	1.6
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OS PARCEL 244		1.6	1.6
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OS PARCEL 255		1.6	1.6
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OS PARCEL 257		1.6	1.6
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OS PARCEL 260		1.6	1.6
OS PARCEL 261		1.6	1.6
OS PARCEL 262		1.6	1.6
OS PARCEL 263		1.6	1.6
OS PARCEL 264		1.6	1.6
OS PARCEL 265		1.6	1.6
OS PARCEL 266		1.6	1.6
OS PARCEL 267		1.6	1.6
OS PARCEL 268		1.6	1.6
OS PARCEL 269		1.6	1.6
OS PARCEL 270		1.6	1.6
OS PARCEL 271		1.6	1.6
OS PARCEL 272		1.6	1.6
OS PARCEL 273		1.6	1.6
OS PARCEL 274		1.6	1.6

TENTATIVE MAP FOR GRAY'S CROSSING - UNIT 1

BEING SECTION 2 AND A PORTION OF THE N. 1/4 SECTION 8, TOWNSHIP 17
NORTH, RANGE 6 EAST, M.D.M. IN THE TOWN OF TRUCKEE, CALIFORNIA



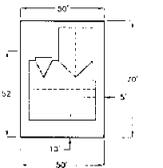
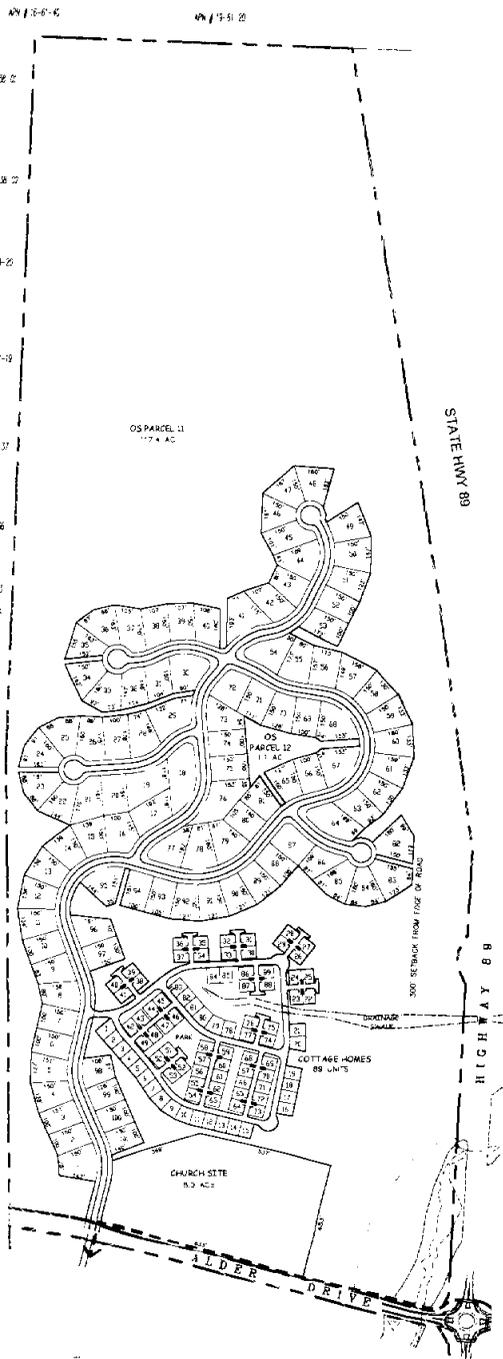
SITE INDEX MAP
N.T.S.

RESIDENTIAL LOTS - UNIT 1

LOT NO.	S. 21' W. 1/4	LOT NO.	S. 21' W. 1/4
1	17,898	51	15,308
2	15,000	52	15,306
3	15,300	53	17,727
4	16,216	54	16,224
5	15,306	55	15,293
6	15,307	56	15,882
7	15,300	57	16,278
8	15,491	58	15,300
9	14,248	59	16,600
10	14,248	60	16,600
11	14,248	61	16,600
12	14,248	62	16,600
13	16,308	63	15,350
14	16,308	64	17,068
15	16,308	65	17,150
16	16,308	66	17,150
17	16,308	67	17,150
18	16,308	68	17,150
19	16,308	69	17,150
20	16,308	70	17,150
21	16,308	71	17,150
22	16,308	72	17,150
23	16,858	73	14,451
24	17,082	74	15,000
25	17,082	75	15,000
26	17,082	76	15,000
27	17,082	77	15,000
28	17,082	78	15,000
29	17,082	79	15,000
30	17,082	80	15,000
31	17,082	81	15,000
32	17,082	82	15,000
33	17,082	83	15,000
34	17,082	84	15,000
35	17,082	85	15,000
36	17,082	86	15,000
37	17,082	87	15,000
38	17,082	88	15,000
39	17,082	89	15,000
40	17,082	90	15,000
41	17,082	91	15,000
42	17,082	92	15,000
43	17,082	93	15,000
44	17,082	94	15,000
45	17,082	95	15,000
46	17,082	96	15,000
47	17,082	97	15,000
48	17,082	98	15,000
49	17,082	99	15,000
50	17,082	100	15,000
51	17,082	101	15,000
52	17,082	102	15,000
53	17,082	103	15,000
54	17,082	104	15,000
55	17,082	105	15,000
56	17,082	106	15,000
57	17,082	107	15,000
58	17,082	108	15,000
59	17,082	109	15,000
60	17,082	110	15,000
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62	17,082	112	15,000
63	17,082	113	15,000
64	17,082	114	15,000
65	17,082	115	15,000
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70	17,082	120	15,000
71	17,082	121	15,000
72	17,082	122	15,000
73	17,082	123	15,000
74	17,082	124	15,000
75	17,082	125	15,000
76	17,082	126	15,000
77	17,082	127	15,000
78	17,082	128	15,000
79	17,082	129	15,000
80	17,082	130	15,000
81	17,082	131	15,000
82	17,082	132	15,000
83	17,082	133	15,000
84	17,082	134	15,000
85	17,082	135	15,000
86	17,082	136	15,000
87	17,082	137	15,000
88	17,082	138	15,000
89	17,082	139	15,000
90	17,082	140	15,000
91	17,082	141	15,000
92	17,082	142	15,000
93	17,082	143	15,000
94	17,082	144	15,000
95	17,082	145	15,000
96	17,082	146	15,000
97	17,082	147	15,000
98	17,082	148	15,000
99	17,082	149	15,000
100	17,082	150	15,000

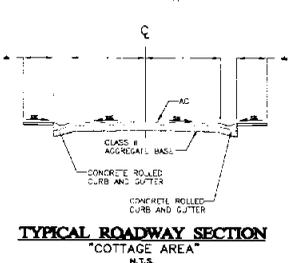
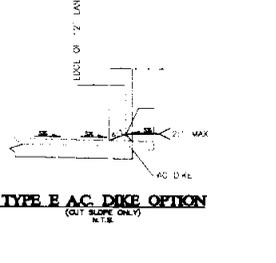
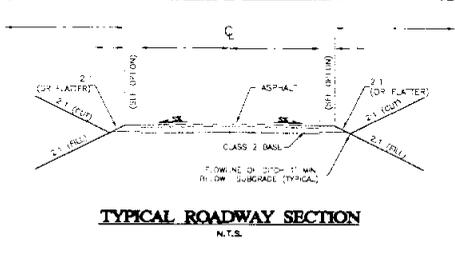
LEGEND:

- WETLANDS (DELINEATED BY ECOSYNTHESIS - OCT. 2001)
- 100 YEAR FLOOD PLAIN
- EXISTING DRAINAGE
- PROPOSED 4' SOFT SURFACE TRAIL
- PROPOSED 8' SEPARATED PAVED SURFACE TRAIL
- PROPOSED 8' CLASS I TRAIL



LAND AREA STATISTICS - UNIT 1

USE	DEVELOPED AREA (AC)	OPEN SPACE AREA (AC)	TOTAL (AC)
CHURCH/PARK	4.0	4.0	8.0
OS PARCEL 11	-	17.4	17.4
OS PARCEL 12	-	1.1	1.1
WETLANDS/PAVED TRAIL	48.3	-	48.3
WETLANDS	-	1.9	1.9
COTTAGE LOTS	9.6	1.9	11.5
TOTAL	61.9	24.4	86.0



3
6

SHAWN KELLEY
CIVIL ENGINEER
TRUCKEE
360 468-4444
360 468-9744

TOWN OF TRUCKEE

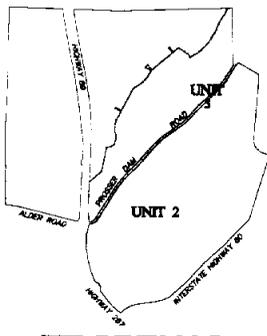
TENTATIVE MAP FOR: GRAY'S CROSSING - UNIT 1 A PLANNED DEVELOPMENT

NO.	REVISIONS	DATE	DESIGNED, DRC
1	PRELIMINARY REVISION	12/20/01	SHAWN KELLEY
2	PRELIMINARY REVISION	12/20/01	PROJ. NO. 0106
3	PRELIMINARY REVISION	12/20/01	DWG. SEC. DAYSTAMP
4	PRELIMINARY REVISION	12/20/01	DATE: SEC. DAYSTAMP

CALIFORNIA

TENTATIVE MAP FOR GRAYS CROSSING - UNITS 2 & 3

BEING SECTION 2 AND A PORTION OF THE N. 1/4 SECTION 11, TOWNSHIP 17
NORTH, RANGE 16 EAST, M.D.M., IN THE TOWN OF TRUCKEE, CALIFORNIA.



SITE INDEX MAP

LEGEND:

- WETLANDS (DELINEATED BY ECOSYNTHESIS - OCT. 2001)
- 100 YEAR FLOOD PLAIN
- EXISTING DRAINAGE
- PROPOSED 4' CLASS II TRAIL
- PROPOSED 4' SOFT SURFACE TRAIL
- PROPOSED 8' SEPARATED PAVED SURFACE TRAIL
- PROPOSED 8' CLASS I TRAIL
- APPROXIMATE GOLF CART PATH ROUTING

LAND AREA STATISTICS - UNITS 2 & 3

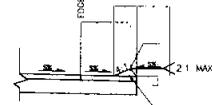
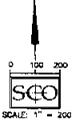
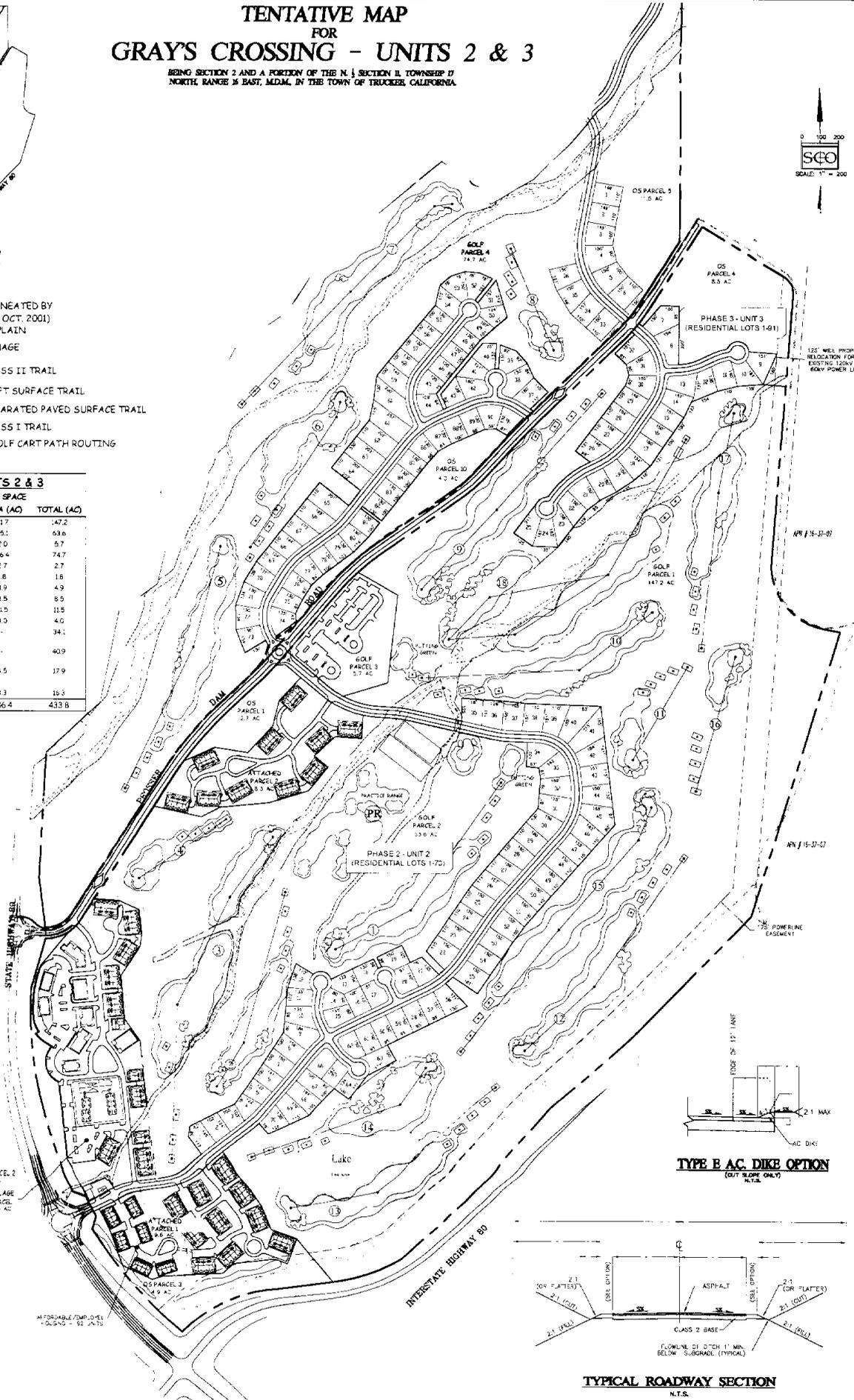
USE	DEVELOPED AREA (AC)	OPEN SPACE AREA (AC)	TOTAL (AC)
GOLF PARCEL 1	72.5	71.7	144.2
GOLF PARCEL 2	28.5	35.1	63.6
GOLF PARCEL 3	3.7	2.0	5.7
GOLF PARCEL 4	18.3	56.4	74.7
OS PARCEL 1		2.7	2.7
OS PARCEL 2		1.8	1.8
OS PARCEL 3		4.9	4.9
OS PARCEL 4		8.5	8.5
OS PARCEL 5		11.5	11.5
OS PARCEL 10		4.0	4.0
RESIDENTIAL LOTS (1/2" x 75')	341		341
RESIDENTIAL LOTS (1/2" x 95')	40.9		40.9
ATTACHED UNITS	13.4	4.5	17.9
VILLAGE PARCEL	13.0	3.3	16.3
TOTAL	227.4	206.4	433.8

RESIDENTIAL LOTS - UNIT 2

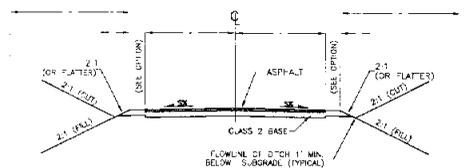
LOT NO.	SIZE (SQ FT)	AREA (AC)	AREA (SQ FT)
1	5,867	0.134	5,867
2	5,867	0.134	5,867
3	5,867	0.134	5,867
4	5,867	0.134	5,867
5	5,867	0.134	5,867
6	5,867	0.134	5,867
7	5,867	0.134	5,867
8	5,867	0.134	5,867
9	5,867	0.134	5,867
10	5,867	0.134	5,867
11	5,867	0.134	5,867
12	5,867	0.134	5,867
13	5,867	0.134	5,867
14	5,867	0.134	5,867
15	5,867	0.134	5,867
16	5,867	0.134	5,867
17	5,867	0.134	5,867
18	5,867	0.134	5,867
19	5,867	0.134	5,867
20	5,867	0.134	5,867
21	5,867	0.134	5,867
22	5,867	0.134	5,867
23	5,867	0.134	5,867
24	5,867	0.134	5,867
25	5,867	0.134	5,867
26	5,867	0.134	5,867
27	5,867	0.134	5,867
28	5,867	0.134	5,867
29	5,867	0.134	5,867
30	5,867	0.134	5,867
31	5,867	0.134	5,867
32	5,867	0.134	5,867
33	5,867	0.134	5,867
34	5,867	0.134	5,867

RESIDENTIAL LOTS - UNIT 3

LOT NO.	SIZE (SQ FT)	AREA (AC)	AREA (SQ FT)
1	6,443	0.148	6,443
2	6,443	0.148	6,443
3	6,443	0.148	6,443
4	6,443	0.148	6,443
5	6,443	0.148	6,443
6	6,443	0.148	6,443
7	6,443	0.148	6,443
8	6,443	0.148	6,443
9	6,443	0.148	6,443
10	6,443	0.148	6,443
11	6,443	0.148	6,443
12	6,443	0.148	6,443
13	6,443	0.148	6,443
14	6,443	0.148	6,443
15	6,443	0.148	6,443
16	6,443	0.148	6,443
17	6,443	0.148	6,443
18	6,443	0.148	6,443
19	6,443	0.148	6,443
20	6,443	0.148	6,443
21	6,443	0.148	6,443
22	6,443	0.148	6,443
23	6,443	0.148	6,443
24	6,443	0.148	6,443
25	6,443	0.148	6,443
26	6,443	0.148	6,443
27	6,443	0.148	6,443
28	6,443	0.148	6,443
29	6,443	0.148	6,443
30	6,443	0.148	6,443
31	6,443	0.148	6,443
32	6,443	0.148	6,443
33	6,443	0.148	6,443
34	6,443	0.148	6,443
35	6,443	0.148	6,443
36	6,443	0.148	6,443
37	6,443	0.148	6,443
38	6,443	0.148	6,443
39	6,443	0.148	6,443
40	6,443	0.148	6,443
41	6,443	0.148	6,443
42	6,443	0.148	6,443
43	6,443	0.148	6,443
44	6,443	0.148	6,443
45	6,443	0.148	6,443



TYPE B AC DIKE OPTION
(OUT SLOPE ONLY)
N.T.S.



TYPICAL ROADWAY SECTION
N.T.S.

4 OF 6
 GRAY VALLEY
 1000 N. MAIN ST.
 TRUCKEE, NV 89406
 TEL: 775-443-1000
 FAX: 775-443-0700

TENTATIVE MAP FOR:
GRAYS CROSSING - UNITS 2 & 3
 A PLANNED DEVELOPMENT
 TOWN OF TRUCKEE
 CALIFORNIA

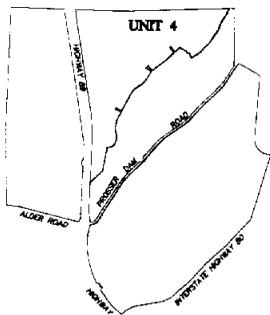
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2	APPROVAL REVISION	12/7/00	DRWN: MCE

DATE: 1/17/00
 DRAWN: MCE
 CHECK: MCE
 DATE: 12/7/00
 DRWN: MCE
 CHECK: MCE
 DATE: 1/17/00
 DRWN: MCE
 CHECK: MCE

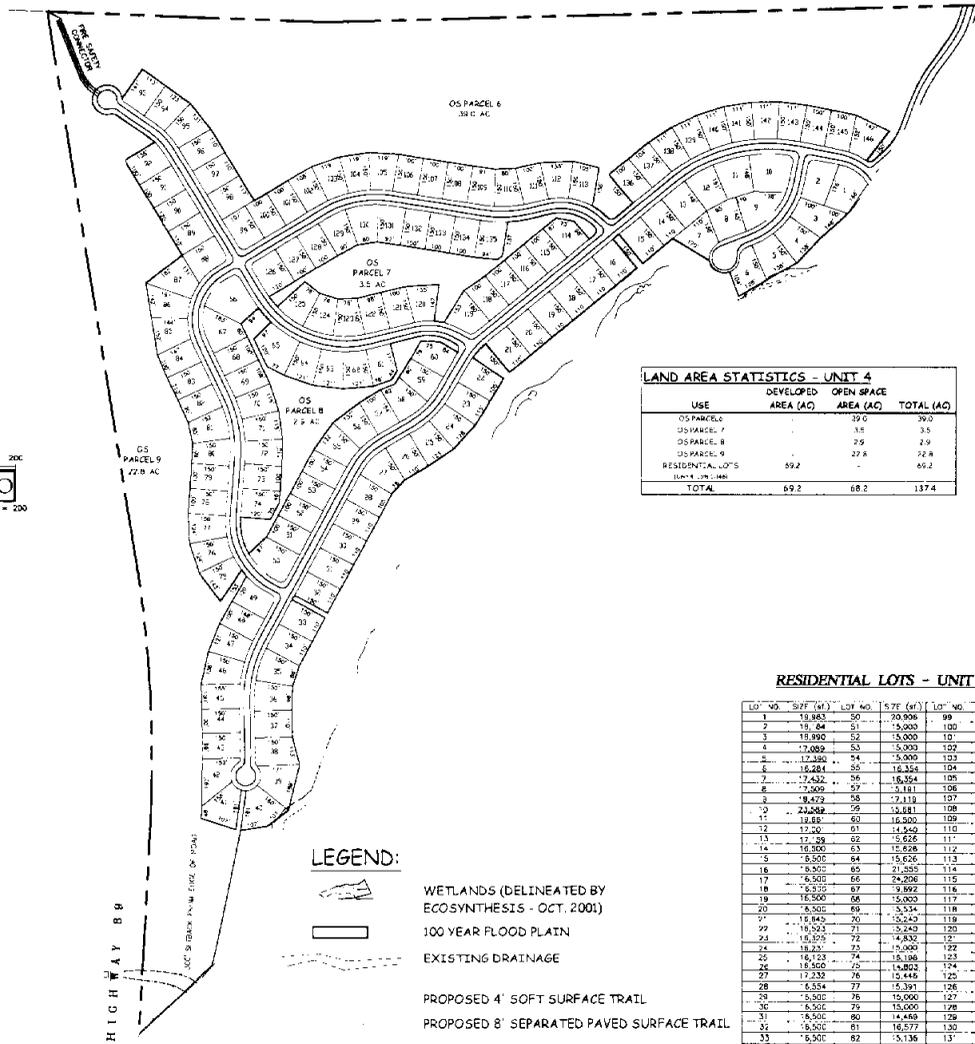
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TENTATIVE MAP FOR GRAYS CROSSING - UNIT 4

BEING SECTION 2 AND A PORTION OF THE N. 1/4 SECTION 11, TOWNSHIP 17
NORTH, RANGE 16 EAST, M.D.M., IN THE TOWN OF TRUCKEE, CALIFORNIA.



SITE INDEX MAP
N.T.S.



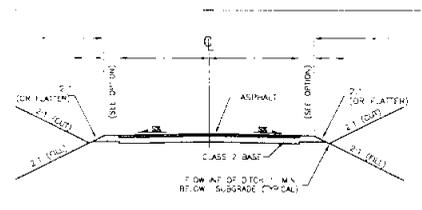
USE	DEVELOPED AREA (AC)	OPEN SPACE AREA (AC)	TOTAL (AC)
OS PARCEL 6	19.0	39.0	
OS PARCEL 7	3.5	3.5	
OS PARCEL 8	2.9	2.9	
OS PARCEL 9	22.8	22.8	
RESIDENTIAL LOTS	69.2	-	69.2
TOTAL	69.2	68.2	137.4

RESIDENTIAL LOTS - UNIT 4

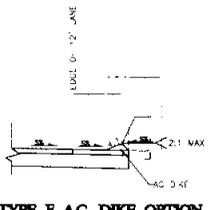
LOT NO.	SIZE (sq ft)	LOT NO.	SIZE (sq ft)	LOT NO.	SIZE (sq ft)
1	18,883	50	20,908	99	15,819
2	18,884	51	18,000	100	15,000
3	18,890	52	18,000	101	18,000
4	17,089	53	18,000	102	18,148
5	17,386	54	18,000	103	18,248
6	15,284	55	16,324	104	18,248
7	17,432	56	16,324	105	18,248
8	17,309	57	18,111	106	18,000
9	9,472	58	17,119	107	18,000
10	22,242	59	18,111	108	18,000
11	18,888	60	18,000	109	15,148
12	17,200	61	14,848	110	18,188
13	17,200	62	15,616	111	18,248
14	16,800	63	15,624	112	18,248
15	6,500	64	15,624	113	18,188
16	6,500	65	21,808	114	18,848
17	6,500	66	24,208	115	18,200
18	6,320	67	9,892	116	18,200
19	16,500	68	18,000	117	18,000
20	6,500	69	5,334	118	18,500
21	18,848	70	18,248	119	18,388
22	18,592	71	9,248	120	18,518
23	18,576	72	4,832	121	18,500
24	18,224	73	18,000	122	18,000
25	16,128	74	18,188	123	18,531
26	18,600	75	12,448	124	18,531
27	17,232	76	18,000	125	18,531
28	6,534	77	15,391	126	7,939
29	6,500	78	18,000	127	18,000
30	6,500	79	18,000	128	18,000
31	6,500	80	14,469	129	18,204
32	6,500	81	16,877	130	18,837
33	6,500	82	15,136	131	18,323
34	15,498	83	14,999	132	18,000
35	17,969	84	4,798	133	18,200
36	18,778	85	18,000	134	18,200
37	18,576	86	17,417	135	18,418
38	18,778	87	18,000	136	18,000
39	18,076	88	7,158	137	18,200
40	20,481	89	18,000	138	14,372
41	18,831	90	18,000	139	18,272
42	17,767	91	18,500	140	4,872
43	18,045	92	18,000	141	4,872
44	18,000	93	18,500	142	4,872
45	18,137	94	18,577	143	4,872
46	17,613	95	18,208	144	18,000
47	18,848	96	18,500	145	18,014
48	14,882	97	18,500	146	18,185
49	15,287	98	17,585		

LEGEND:

- WETLANDS (DELINEATED BY ECOSYNTHESIS - OCT. 2001)
- 100 YEAR FLOOD PLAIN
- EXISTING DRAINAGE
- PROPOSED 4' SOFT SURFACE TRAIL
- PROPOSED 8' SEPARATED PAVED SURFACE TRAIL



TYPICAL ROADWAY SECTION
N.T.S.



TYPE E A.C. DIKE OPTION
(OUT SLOPE ONLY)
N.T.S.

5
OF
6

GRANT TAYLOR
800 578-8841
TRUCKEE
800 580-8848
FAX: 800 878-8880

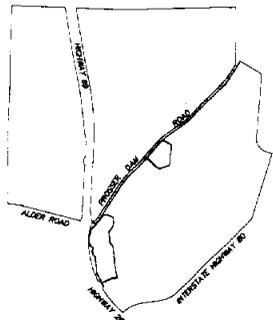
TENTATIVE MAP FOR:
GRAYS CROSSING - UNIT 4
A PLANNED DEVELOPMENT

NO.	REVISIONS	DATE	DESIGNED: DFC
1	PRELIMINARY REVISION	1/2/03	DAWN MCR
2	INCREASED DENSITY	10/17/03	PROJ. NO. 0108
			DWG. SEC. DAYSTAMP
			DATE: SEE DAYSTAMP

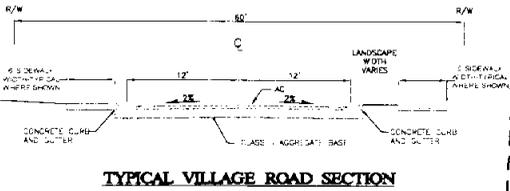
CALIFORNIA

TOWN OF TRUCKEE

TENTATIVE MAP FOR GRAY'S CROSSING - VILLAGE AREA & GOLF PARCEL 3



SITE INDEX MAP
N.T.S.



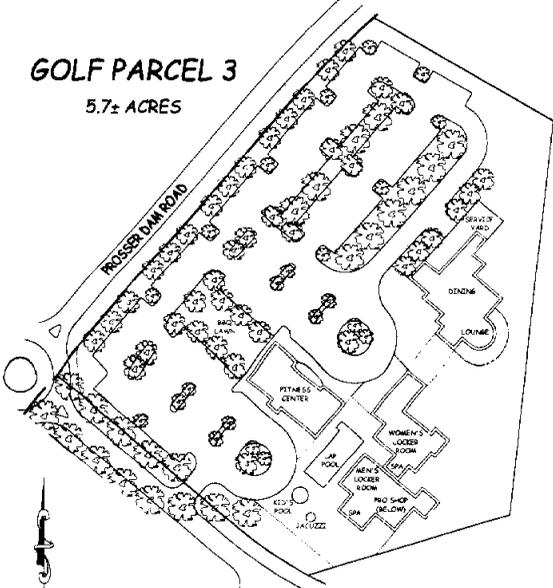
TYPICAL VILLAGE ROAD SECTION
N.T.S.

- LEGEND:**
- 100 YEAR FLOOD PLAIN
 - EXISTING DRAINAGE
 - PROPOSED 4' CLASS II TRAIL
 - PROPOSED 4' SOFT SURFACE TRAIL
 - PROPOSED 8' CLASS I TRAIL

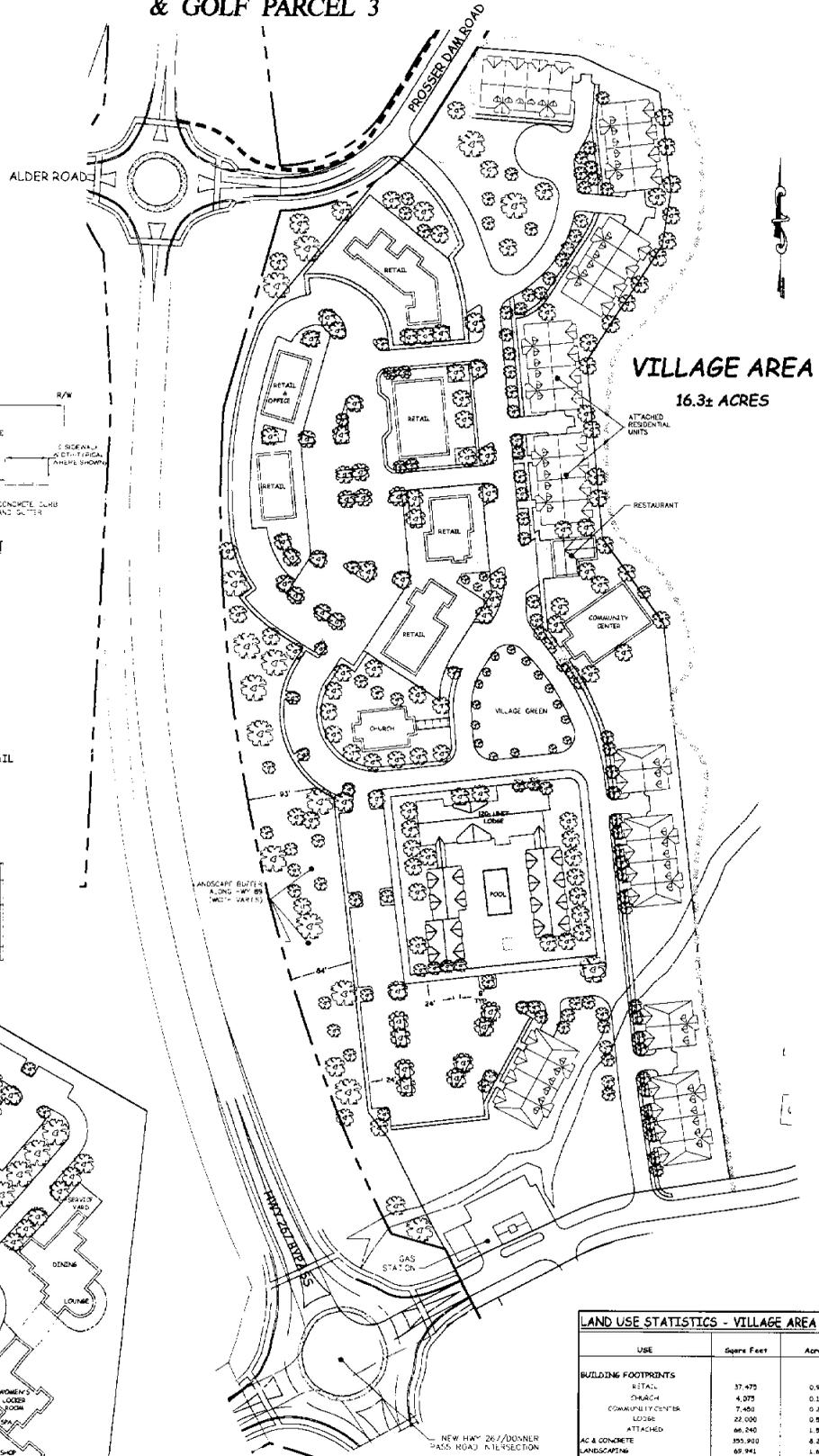
LAND USE STATISTICS - GOLF PARCEL 3

USE	Square Feet	Acres	% OF TOTAL
BUILDING FOOTPRINTS	75,568	0.6	11%
AC & CONCRETE	128,114	2.9	52%
LANDSCAPING	8,337	0.2	3%
OPEN SPACE	82,100	2.0	24%
TOTAL	248,014	5.7	100%

GOLF PARCEL 3
5.7± ACRES



GOLF PARCEL 3
SCALE: 1" = 60'



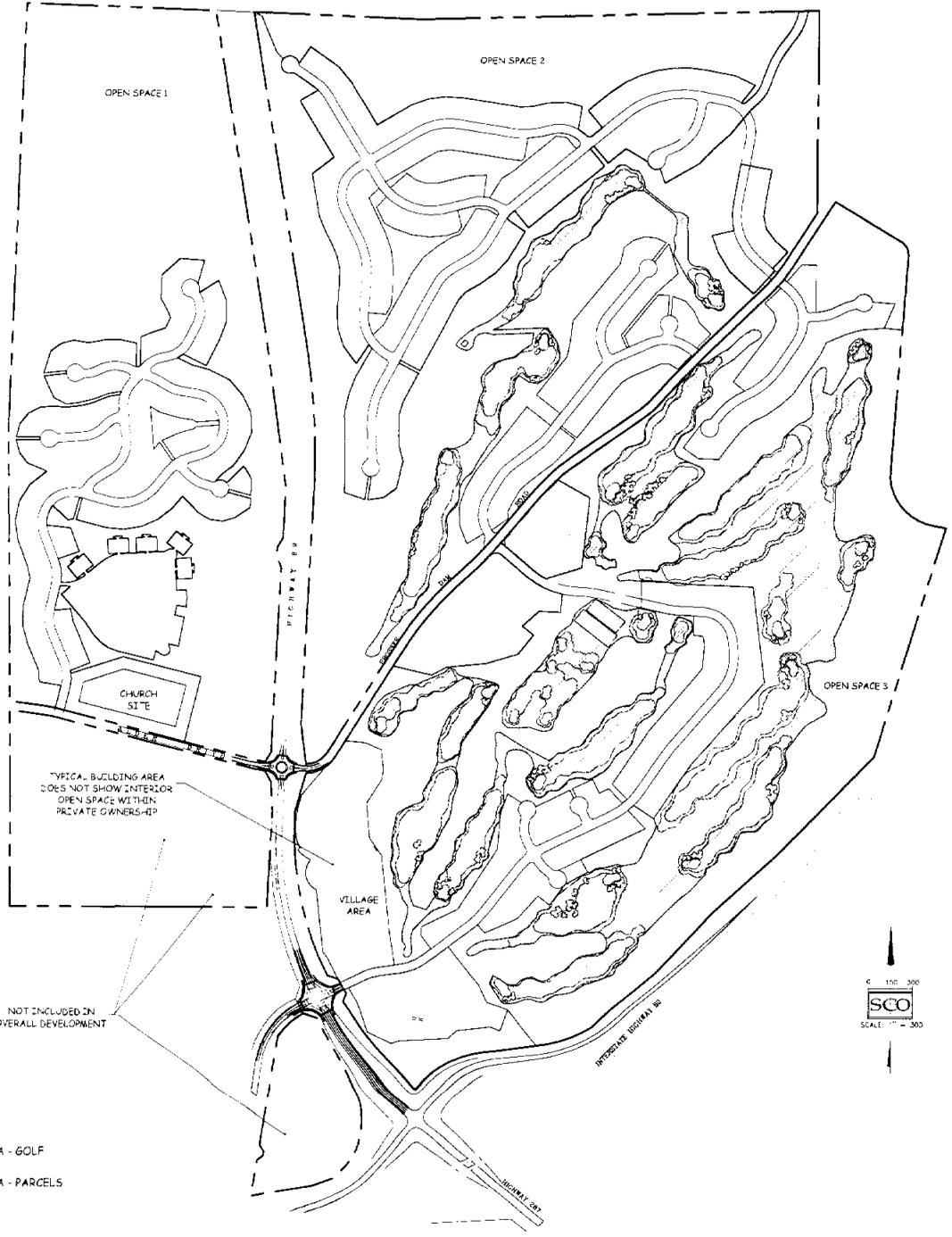
VILLAGE AREA
16.3± ACRES

LAND USE STATISTICS - VILLAGE AREA

USE	Square Feet	Acres	% OF TOTAL
BUILDING FOOTPRINTS			
RETAIL	37,473	0.9	5%
CHURCH	4,373	0.1	1%
COMMUNITY CENTERS	7,460	0.2	1%
LODGE	22,000	0.5	3%
ATTACHED	66,240	1.5	9%
AC & CONCRETE	151,910	3.2	50%
LANDSCAPING	69,941	1.6	10%
OPEN SPACE	145,025	3.3	20%
TOTAL	708,106	16.3	94%

VILLAGE AREA
SCALE: 1" = 60'

DEVELOPMENT AREA FOR GRAY'S CROSSING



- LEGEND:**
- DEVELOPED AREA - GOLF
 - DEVELOPED AREA - PARCELS
 - OPEN SPACE

NOT INCLUDED IN OVERALL DEVELOPMENT

SCALE: 1" = 300'

UNION VALLEY
1501 472 5811
TRUCKEE
432 545-1042
FAX 509-472-1880



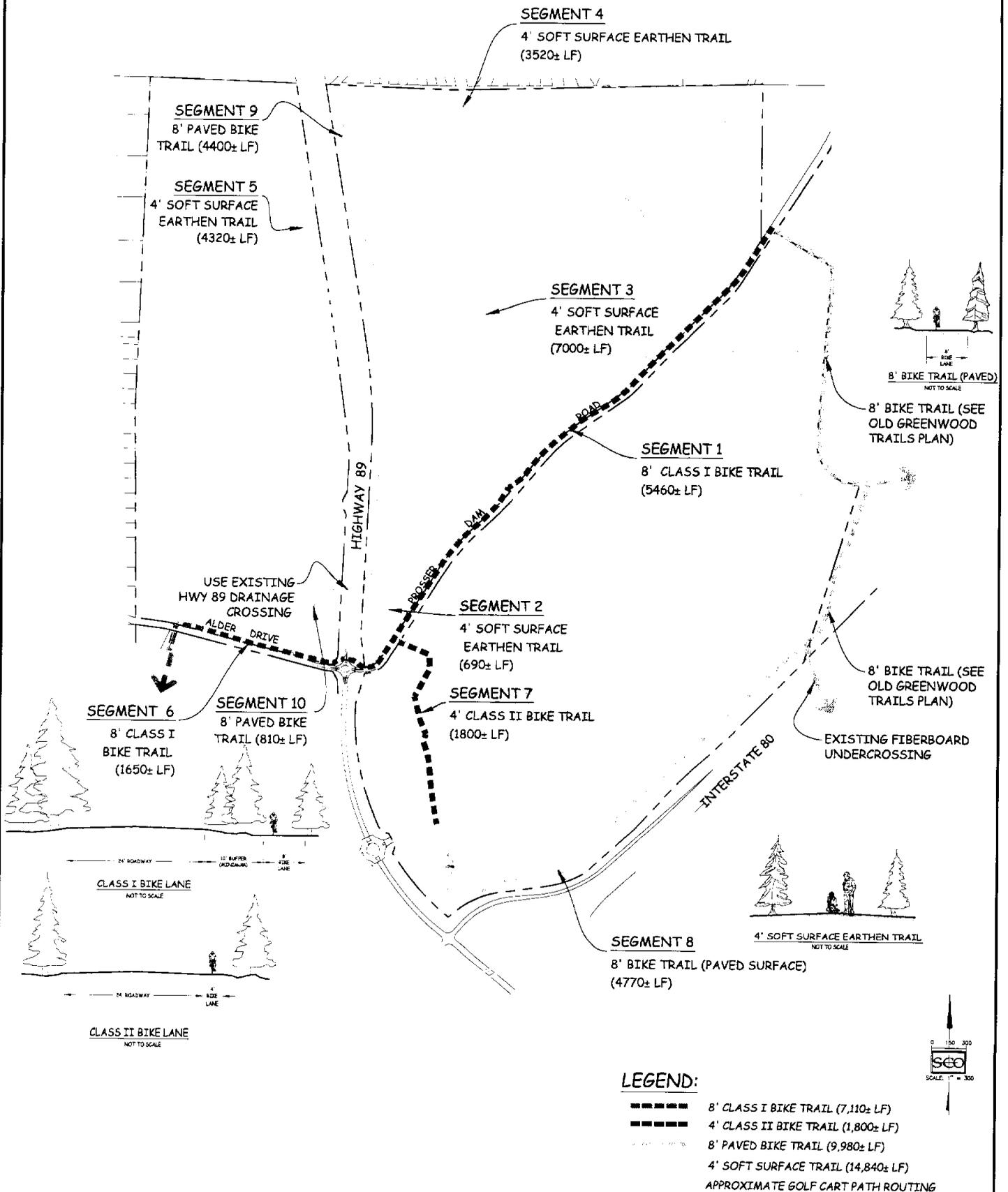
TOWN OF TRUCKEE

OVERALL DEVELOPMENT PLAN FOR: **GRAY'S CROSSING** A PLANNED DEVELOPMENT

CALIFORNIA

NO.	REVISIONS	DATE	DESIGNED BY
1	FLOORPLAN REVISIONS	1/2/03	DRAWN: MGR
2	INCREASES IN LOTTY	11/28/03	PROJ. NO. 0106
			DWG. SEE DAYSTAMP
			DATE: 1/14/04

GRAY'S CROSSING TRAILS PLAN

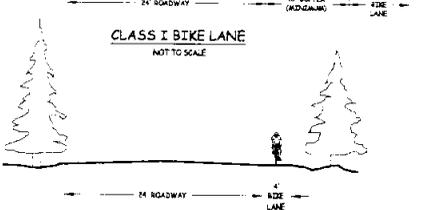
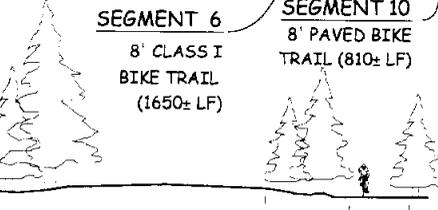


8' BIKE TRAIL (SEE OLD GREENWOOD TRAILS PLAN)

8' BIKE TRAIL (SEE OLD GREENWOOD TRAILS PLAN)

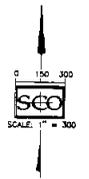


SEGMENT 8
8' BIKE TRAIL (PAVED SURFACE) (4770± LF)



LEGEND:

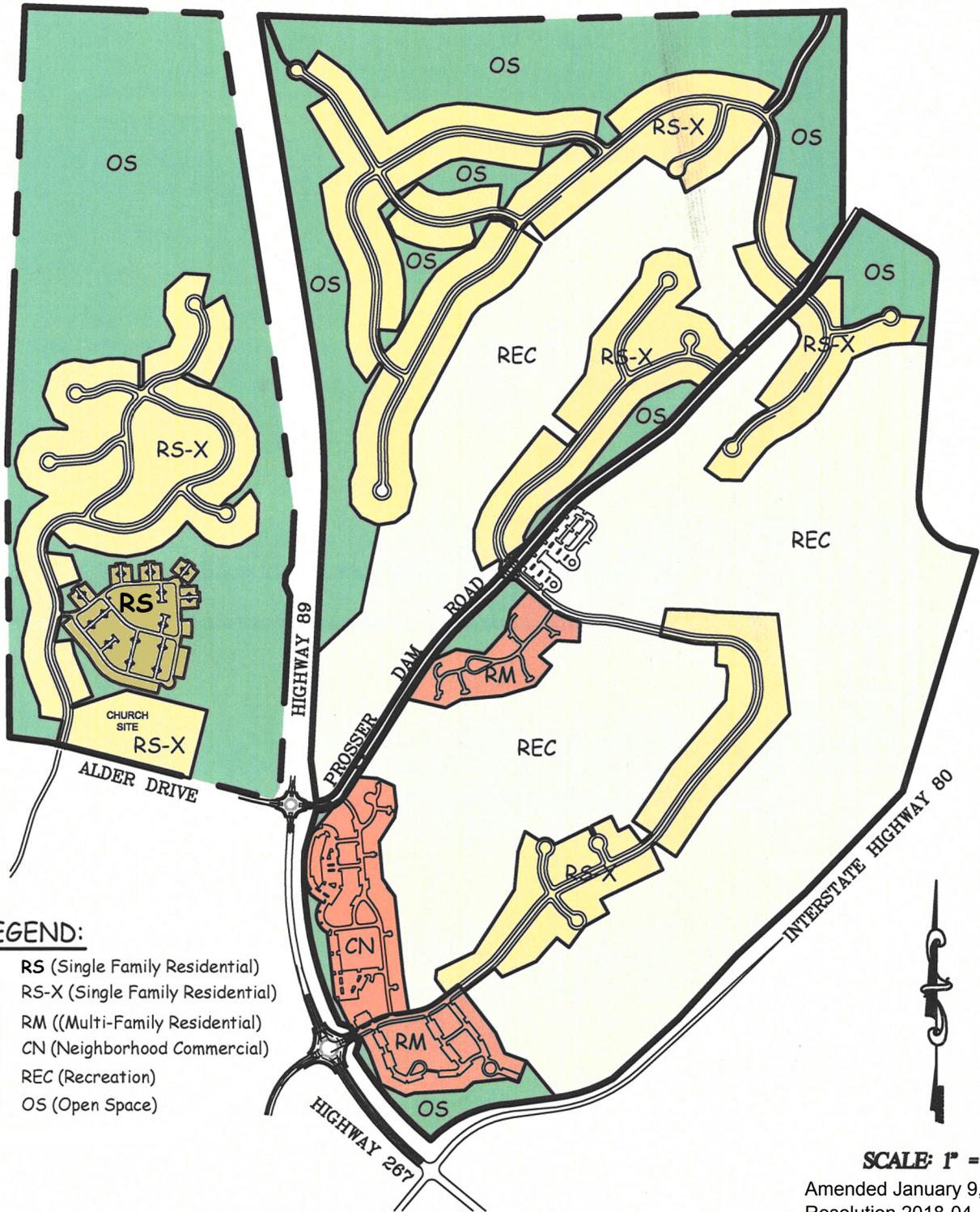
- 8' CLASS I BIKE TRAIL (7,110± LF)
- 4' CLASS II BIKE TRAIL (1,800± LF)
- 8' PAVED BIKE TRAIL (9,980± LF)
- 4' SOFT SURFACE TRAIL (14,840± LF)
- APPROXIMATE GOLF CART PATH ROUTING



NO.	REVISIONS	DATE	DESIGNED BY

ZONING EXHIBIT MAP

FOR
GRAY'S CROSSING



LEGEND:

- RS (Single Family Residential)
- RS-X (Single Family Residential)
- RM ((Multi-Family Residential)
- CN (Neighborhood Commercial)
- REC (Recreation)
- OS (Open Space)

SCALE: 1" = 100'

Amended January 9, 2018
Resolution 2018-04

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1
OF
1

GRASS VALLEY
530-272-8841
TRUCKEE
530-582-4043
FAX: 530-282-5736



TOWN OF TRUCKEE

GRAYS CROSSING Zoning Exhibit Map

CALIFORNIA

NO.	REVISIONS	DATE	DESIGNED: raw
			DRAWN: raw
			PROJ. NO: 0106
			DWG: See Daystamp
			DATE: See Daystamp

Executive Summary

CHAPTER I EXECUTIVE SUMMARY

PROJECT DESCRIPTION

The proposed Gray's Crossing Specific Plan is a destination recreation community proposing golf, recreational amenities, single-family lots, cottage homes, attached housing, and mixed use development in a village center setting, with two community church sites and over 417 acres of passive open space. Gray's Crossing property is located north of Interstate 80 and on both sides of State Highway 89 North. The 757-acre property has a variety of topographical and vegetative conditions ranging from meadows to heavily timbered hillsides. The location, topography and acreage allow the various proposed land uses to be sited in a manner that maintains the environmental integrity of the surrounding area, Interstate 80, State Highway 89 and Prosser Dam Road Corridors.

The Gray's Crossing Specific Plan has been designed to be developed as a Planned Community allowing a variety of land uses. The land use plans incorporates a variety of housing types, a village center, recreational amenities, and open space. Each of these land uses are interrelated to the project as a whole.

The following describes each of the specific Land Use Districts:

Residential District

The purpose of the Residential District is to provide land use area for residential uses that allow for single-family custom built homes, cottage homes, and related accessory uses.

The cottage-housing concept is designed to create a more economic housing opportunity by efficiently using land area and reducing the infrastructure needed to serve single-family residential development. The basic concept is to minimize individual land ownership around a parcel and share in common courtyards or recreation areas designed to service individual clusters of four (4) to eight (8) units.

Attached Residential District

The purpose and intent of the Attached Residential District are to provide residential land for medium density residential, a 92-unit affordable and employee housing complex and their accessory uses.

Village Center District

The Specific Plan District for a mixed-use Village Center is intended to provide land area for a range of retail, office, lodging services and business activity relating to the needs of the recreational activities and surrounding neighborhoods. The project proposes retail and office uses, lodging units, community facilities, church facilities, and residential loft units above the retail shops.

Recreation District

The Recreation District is intended to provide tourist oriented land uses ranging from country club and restaurant, health and fitness center, 18-hole golf course, driving range, and golf maintenance facilities, to a middle school with playgrounds and pedestrian/bike trails.

Open Space District

The Open Space District is intended to provide land area devoted to maintaining the scenic beauty and natural characteristics of the area. The Open Space District is also intended to provide for public and private recreational uses, provided that the integrity of the areas natural characteristics is maintained. Lands within Open Space Districts will be separated from other Districts by dedication to Land Trust, conservation easements, zoning restrictions and or other programs that will ensure both preservation and maintenance of the property.

The Gray's Crossing Specific Plan recognizes different types of Open Space. They are: 1.) Open Space set aside in large parcels which can be maintained by Property Owners' Associations, transferred to Land Trusts or public entities, and/or reserved by conservation easements; and 2.) Open space on private ownerships which is restricted from development, such as rear yard setbacks in the Residential District, open space buffers in the Village Center and Attached Residential Districts, and lands within the Golf Parcels not devoted to fairway construction.

The project proposes the following:

Residential Land Uses:

A tentative map proposing:

- 410 single-family residential lots proposed to be developed in four phases;
- 57 cottage units proposed to be developed in Phase I;
- 48 attached housing units, in a village setting, ranging from studio flats to 3- and 4-bedroom townhomes; and
- 92-unit affordable and employee housing complex, comprised of 64 units of Gray's Crossing affordable housing and 28 units of Old Greenwood employee housing.

Village Complex:

A Conditional Use Permit and Land Development Plan proposing a 47,900 square foot Village and Lodge that allows for the following:

- Office and Retail – 38,900 square feet of office and retail floor space. This area would also accommodate 21 live-work lofts above the retail floor.
- Lodge – 120 rooms with 5,000 square feet of conference facilities and limited food service. The Lodge would be constructed over a ten-year period. Each unit within the

Lodge will be designated to allow for flexible use such as commercial condominiums, time-share or fractional ownership.

- Community Cultural Center – A 7,200 square foot multi-use building that can accommodate worship services, community gatherings, cultural events and theater.
- Gas Station – A 1,800 square foot service station and convenience store.
- Attached Housing – See Residential Land Uses above.
- Church Site – a one-acre site that could provide a future worship center and multi-use facility for the local Truckee Ecumenical Partnership.

Golf Course:

A Conditional Use Permit proposes an 18-hole championship golf course, practice range, maintenance facility, fitness center and clubhouse with restaurant and lounge. The golf facility will be constructed on four parcels with 193 acres of turf and rough area.

Open Space:

Approximately 417 acres of open space is dispersed throughout the project and is connected by a series of trails that network throughout the development and neighboring properties.

Church Site:

This approximately eight (8) acre site could provide a future worship center and related facilities for the local Roman Catholic Church.

DEVELOPMENT PERMIT PROCESSING

The Specific Plan for Gray's Crossing, adopted by Town of Truckee Resolution No. 2003-56, establishes the various land uses, zones, building intensity and design standards for development of properties within the Specific Plan area. Specific design features for various uses within the Specific Plan area still require permit approval by the Town. When development is proposed for the village area, the attached housing areas, the employee housing facilities, and the clubhouse and fitness center facilities, specific building design review and site planning is required to ensure the design proposed complies with the provisions of this Specific Plan, the conditional approval and the Gray's Crossing Development Agreement.

The Gray's Crossing Specific Plan sets forth design guidelines to monitor and direct the development within each specific zone. An Owners' Association will be established and provisions and enforcement criteria will be set forth in the Conditions, Covenants and Restrictions (CC&R's) and will be applicable to all properties within the Specific Plan area. The Owners' Association will first review the proposed development within the Specific Plan area for compliance with the provisions set forth herein. Once the Owners' Association has approved the design for conformance with the Gray's Crossing Specific Plan, application to the Town's Community Development Department, in conformance with Article 4 of the Development Code, will be required. In addition, compliance with Title 15 of the Municipal Code for building construction is also applicable to development within the Specific Plan area. Standards for the development permit application and timing of processing can be found in Title 18 of the Truckee Development Code and at the Community Development Department.

Permit and approval discretion for all development permits with the Specific Plan area is limited to conformance and compliance with the provisions of the Gray's Crossing Specific Plan, the Gray's Crossing Conditional Approval (Resolution No. 2003-22) and the Gray's Crossing Development Agreement (Ordinance Nos. 203-07 and 203-08).

CONSTRUCTION PHASING

The Gray's Crossing project is proposed to be constructed in four units over a six-year period. Development is proposed as follows:

PHASE I

(construction time 2004-2006)

Summary of Construction:

101 Lots, Unit 1
89 Cottage Lots
70 Lots, Unit 2
Utilities
Golf Course and Maintenance Facility
25,000 square feet of Village Center
7 Residential Lofts
30 Attached Units
92 Units Affordable/Employee Housing Complex
40 Lodging Units
Trail Segments 1, 2, 4, 5, 6, 7, & 8

Details of Construction:

- Construct off-site sewer from the southeast corner of the site connecting to the Glenshire Outfall Station that connects to the Truckee Sanitation Agency facility.
- Relocate the north-south 60-KV and 120-KV transmission lines to east of the subject property.
- Combine a 52-acre parcel along the east property line to allow for Golf Holes #16 and #17, the tee boxes for Golf Holes #12 and #18, and 10 single-family residential units.
- Construct the 18-hole championship golf course, practice range, maintenance facility and irrigation ponds.
- Construct 25,000 square feet of the office and retail space and the Service Station in the Village Center.
- Construct 7 residential loft units.
- Construct 40 attached units.

PHASE II

(start construction 2006-2009)

Summary of Construction:

91 Single-Family Lots, Unit 3
10,000 - 15,000 square feet of Village Center
43 Attached Units
11 Residential Loft Units
Golf Amenities
Church Site
40 Lodging Units

Details of Construction:

- Construct golf clubhouse, fitness center and cart barn.
- Provide a church site in the Village Center working through the Truckee Ecumenical Partnership.
- Construct 40 lodging units.
- Pay or construct offsite trail to Downtown.

PHASE III

(start construction 2007-2009)

Summary of Construction:

146 Lots, Unit 4

Details of Construction:

- Complete Village Center and Residential lofts.
- Complete 120-unit Lodge.

Permitted Uses and General Development Standards

CHAPTER II PERMITTED LAND USES AND GENERAL DEVELOPMENT STANDARDS

This Chapter addresses the purpose and intent of each Specific Plan District, the permitted, conditionally permitted, and temporary permitted land uses, and the general development standards. Temporary uses may be permitted for short-term activities throughout all land uses within the Specific Plan area, subject to approval of a Temporary Use Permit. Any discrepancy or need for clarification regarding the permissibility of a land use within a specific District shall be determined by the Town of Truckee Community Development Director.

Specific Plan Districts are:

- Residential District (single-family and cottage homes);
- Attached Residential District;
- Village Center District;
- Recreation District; and
- Open Space District.



RESIDENTIAL DISTRICT

The purpose of the Residential District is to provide land use area for residential uses that allow for single-family custom built homes, cottage homes, and related accessory uses. The permitted, conditionally permitted, and temporary permitted land uses and development standards are based on the provisions set forth in the Town of Truckee Development Code, Chapter 18.08, Residential Zoning Districts. To reflect the design standards and use locations required by the Gray's Crossing Specific Plan, the uses listed here are more restrictive than those permitted in the Development Code.



PERMITTED USES – Single-family and Cottage

Residential Uses

1. Single-family dwellings;
2. Secondary residential units; and
3. Cottage, attached and detached.

Agricultural, Resource and Open Space Uses

1. Nature reserves; and
2. Trails, biking and hiking.

Recreation, Education and Public Assembly Uses

1. Outdoor commercial recreation;
2. Parks and playgrounds; and
3. Private residential recreational facilities.

PERMITTED USES – Single-family and Cottage *(continued)*

Service Uses

1. Child day care center;
2. Public buildings and structures; and
3. Public safety and utility facilities;

Transportation, Infrastructure and Communications Uses

1. Bike racks;
2. Storm water retention facilities;
3. Transit stop shelters; and
4. Utility lines.

TEMPORARY PERMITTED USES – Single-family and Cottage

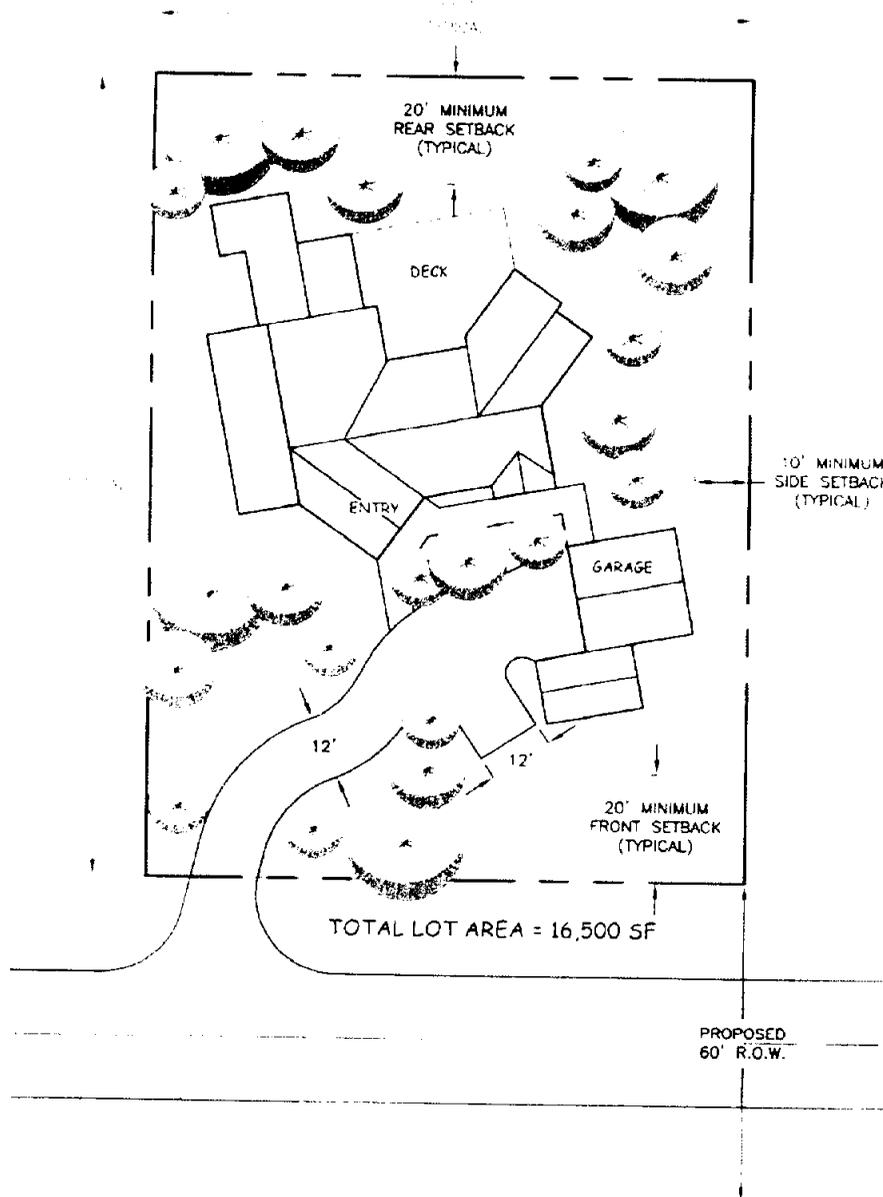
1. Commercial filming;
2. Model homes;
3. Offices, temporary;
4. Offices, temporary real estate; and
5. Temporary work trailers.

GENERAL DEVELOPMENT STANDARDS – Single-Family Homes

1.	Minimum Lot Area	As shown on Tentative Map
2.	Residential Density	One (1) single-family unit per lot and one secondary residential unit in compliance with 18.58.230 of the Truckee Development Code.
3.	Minimum Front Setback	20 feet
4.	Minimum Side Setback	10 feet
5.	Minimum Street Side Setback	15 feet
6.	Minimum Rear Setback	20 feet
7.	Between Structures	As required by Town Building Code
8.	Site Coverage	50% maximum
9.	Maximum Building Height	35 feet or 3 ½ stories whichever is less (as measured from top of roof pitch to midpoint of finished grade and does not include chimney height)
10.	Maximum Garage Height	As required in the RS District
11.	Parking	See Programs, Chapter IV
12.	Minimum Interior Setback	10 feet
13.	Patios/Decks Minimum Exterior Setback	15 feet
14.	Patios/Decks Minimum Interior Setback	6 feet
15.	Swimming Pool/Spa Street Side Setback	15 feet
16.	Swimming Pool/Spa Side and Rear Setback	15 feet
17.	Minimum Driveway Width	12 feet

These setbacks and standards may be adjusted to avoid tree loss or site alternations in accordance with the requirements of the Truckee Development Code.

RESIDENTIAL SINGLE-FAMILY SITE PLAN



Site Statistics:

House Footprint	2,687 SF
Garage and Workshop	666 SF
Deck	589 SF
Surfaced Area	1,745 SF
Total Site Coverage	5,637 SF

Approximately 34%

ACCESSORY USES AND STRUCTURES – Residential

Residential accessory uses include any use that is customarily related to a residence, including garages, greenhouses, storage sheds, studios, swimming pools/spas, second units and workshops.

Accessory uses and structures shall:

1. be incidental to and not alter the character of the site from that created by the main use;
2. comply with the requirements of this Specific Plan applicable to the main structure, including heights, parcel coverage and setbacks;
3. be architecturally compatible with the main structure; and
4. not exceed 1,200-square feet of cumulative floor area of all accessory structures.

Allowable accessory structures shall include:

1. decks, porches, and stairways;
2. detached garages;
3. greenhouses;
4. home occupations;
5. second units, attached and detached;
6. swimming pools/spas/hot tubs; and
7. workshops or studios.

ACCESSORY USES AND STRUCTURES – Residential (continued)

Cottage Homes

The cottage-housing concept is designed to create a more economic housing opportunity by efficiently using land area and reducing the infrastructure needed to serve single-family residential development. The basic concept is to minimize individual land ownership around a parcel and share in common courtyards or recreation areas designed to service individual clusters of four (4) to eight (8) units. Creating an approximately 3,000 square foot lot with fixed guest parking and driveway locations would allow for a 1,200 to 1,300 square foot foundation area. This would allow a one- to two-story unit depending on design and square footage needs of the individual owner. Recreational areas associated with these units would be utilized as common areas by the various owners within each residential pod. Recreational areas would incorporate walkways, access to trailheads, barbecue/picnic areas, and other desirable recreational amenities. The Cottage Home Site Plan (page 15) depicts the clustered concept and courtyard area associated with each unit.



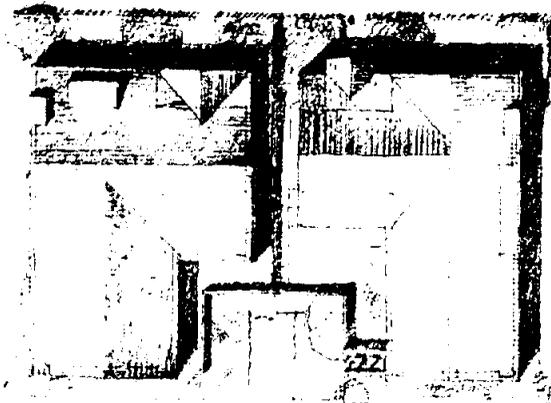
GENERAL DEVELOPMENT STANDARDS – Cottage Homes

- | | | |
|----|-----------------------------|------------------------|
| 1. | Minimum Lot Area | 3,000 square feet |
| 2. | Minimum Lot Width | 50 feet |
| 3. | Residential Density | 1 cottage unit per lot |
| 4. | Minimum Front Setback | 5 feet |
| 5. | Minimum Side Setback | 5 feet |
| 6. | Minimum Street Side Setback | 10 feet |
| 7. | Minimum Rear Setback | 5 feet |

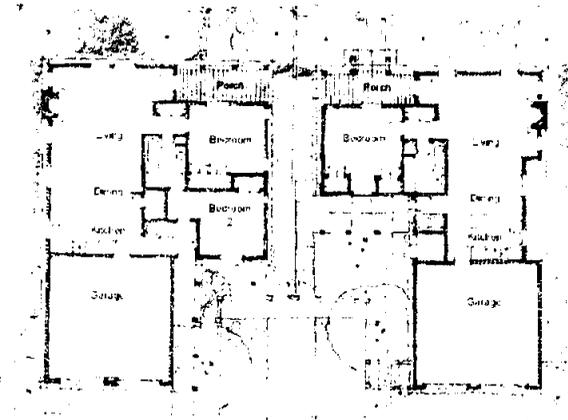
GENERAL DEVELOPMENT STANDARDS – Cottage Homes *(continued)*

8.	Between Detached Structures	10 feet
9.	Site Coverage	80%
10.	Open Space	N/A
11.	Maximum Building Height	24 feet or 2 stories, whichever is less (as measured from top of roof pitch to midpoint of finished grade and does not include chimney height).
12.	Parking	As required by the Truckee Development Code for residential uses, Chapter 18.48.
13.	Minimum Interior Setback	5 feet
14.	Patios/Decks Minimum Exterior Setback	5 feet
15.	Patios/Decks Minimum Interior Setback	5 feet
16.	Minimum Driveway Width	12 feet

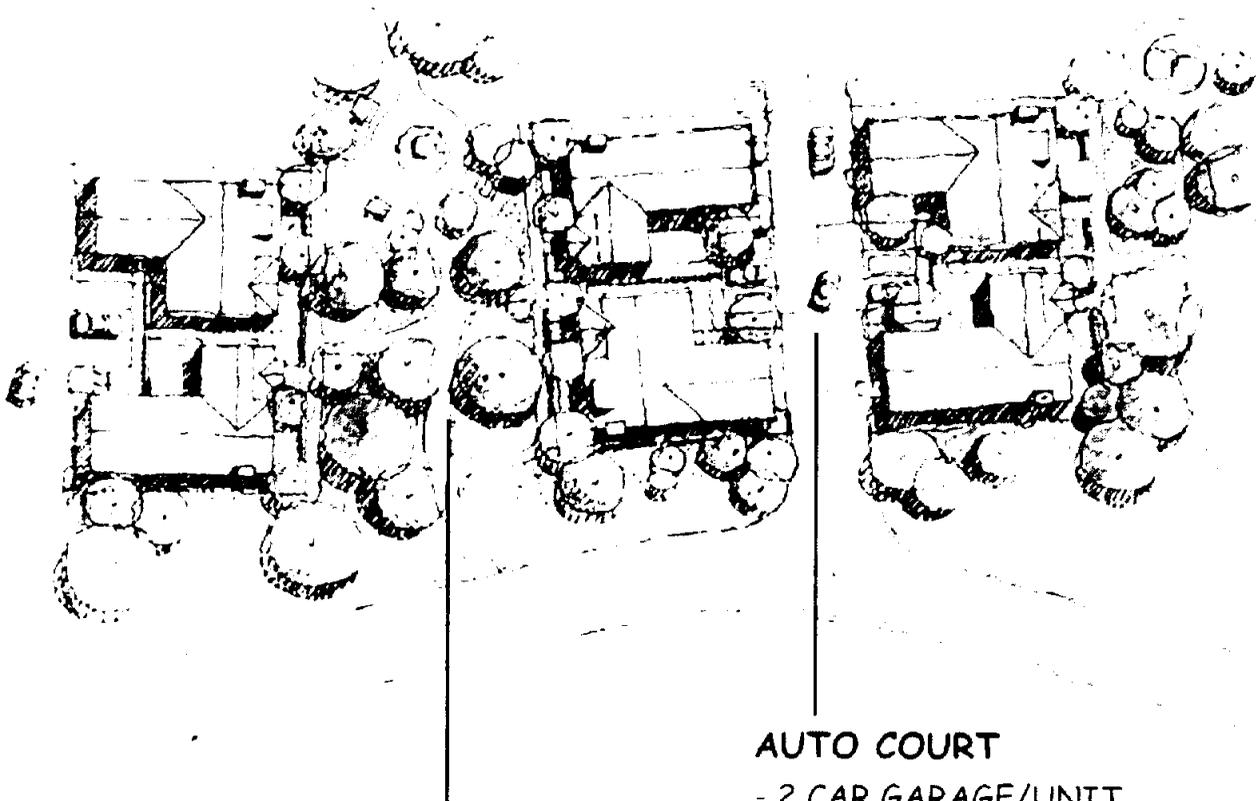
These setbacks and standards may be adjusted to avoid tree loss or site alternations in accordance with the requirements of the Truckee Development Code.



Cottage Homes



COTTAGE HOME SITE PLAN



COURTYARD AREA

- PATH TO EACH UNIT
- RECREATION & BBQ AREA
- LANDSCAPING

AUTO COURT

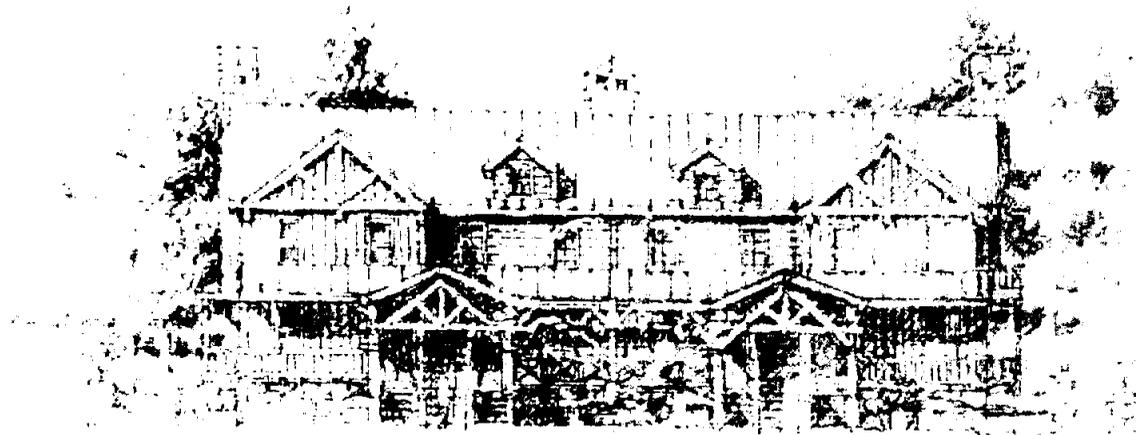
- 2 CAR GARAGE/UNIT
- 1.5 GUEST SPACES/UNIT

* FRONT PORCH FACES COURTYARD

ATTACHED RESIDENTIAL DISTRICT

The purpose and intent of the Attached Residential District are to provide residential land for medium density residential units and their accessory uses. Gray's Crossing will offer detached cottage homes in the Residential District in addition to attached housing in this District.

The permitted, conditionally permitted, and temporary permitted land uses and development standards are based on the provisions set forth in the Town of Truckee Development Code, Chapter 18.08, Residential Zoning Districts. To reflect the design standards and use locations required by the Gray's Crossing Specific Plan, the uses listed here are more restrictive than those permitted in the Development Code.



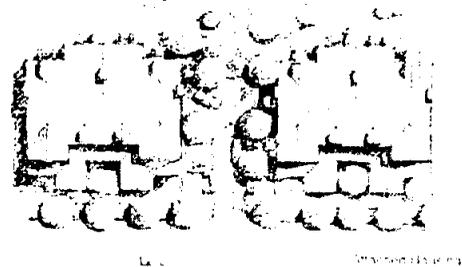
PERMITTED USES – Attached Residential

Residential Uses

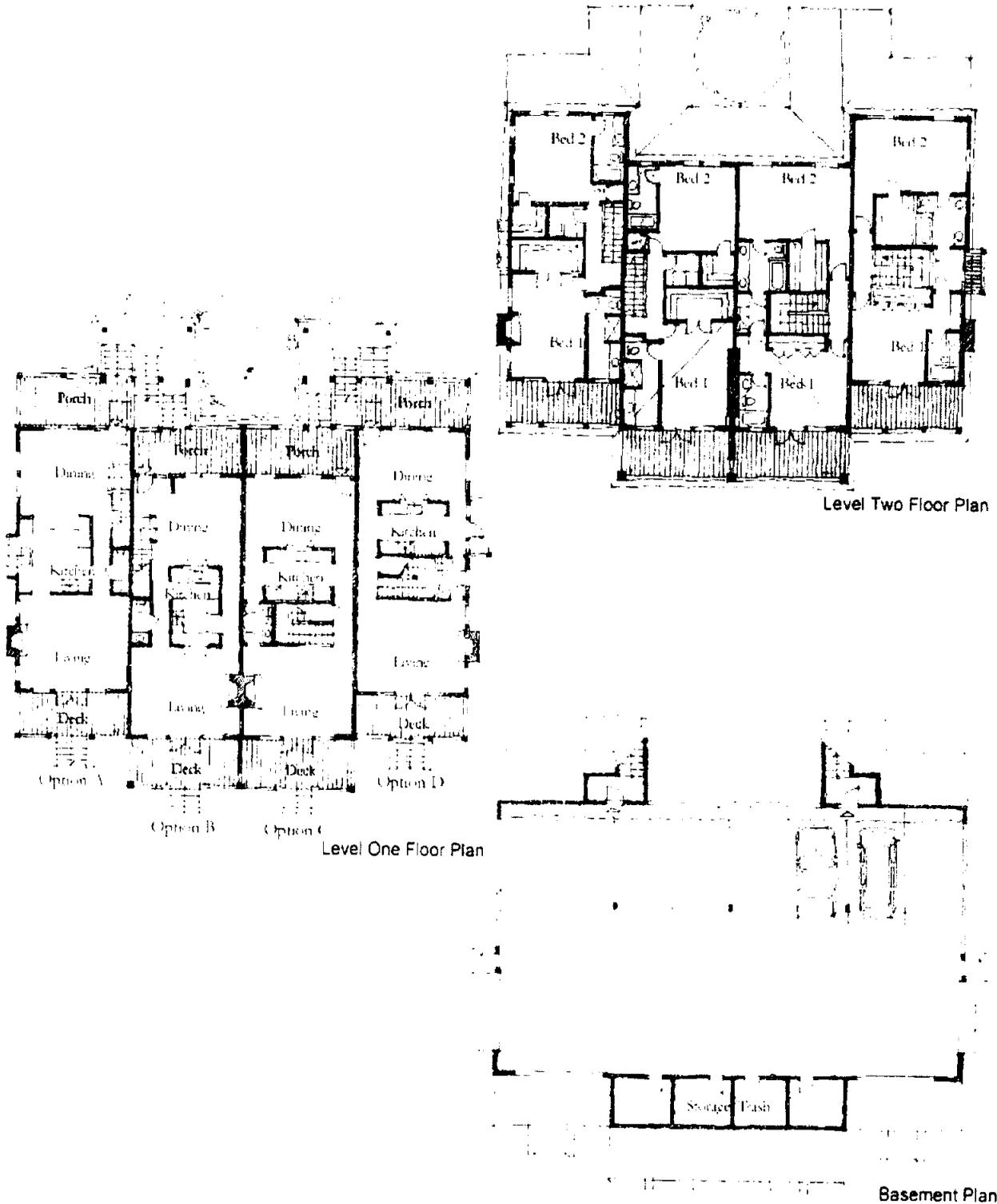
1. Apartments, duplexes, triplexes, townhomes and condominiums;
2. Employee housing;
3. Enclosed and open parking;
4. Multi-unit dwellings;
5. Multi-family dwellings, individual ownership, rental and fractional ownerships;
6. Rooming and boarding houses; and
7. Transient rental.

Agricultural, Resource and Open Space Uses

1. Nature Reserves; and
2. Trails, Biking and Hiking.



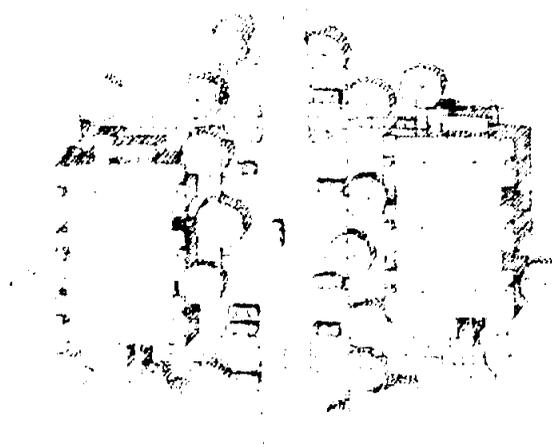
ATTACHED HOUSING FLOOR PLANS



PERMITTED USES – Attached Residential (continued)

Recreation, Education and Public Assembly Uses

1. Community centers;
2. Membership organization facilities;
3. Parks and playgrounds; and
4. Private residential recreational facilities.



Employee Housing Site Plan

Service Uses

1. Public buildings and structures;
2. Public safety and utility facilities; and
3. Storage, accessory to Attached Residential project.

Retail Trade Uses

1. Accessory retail uses.

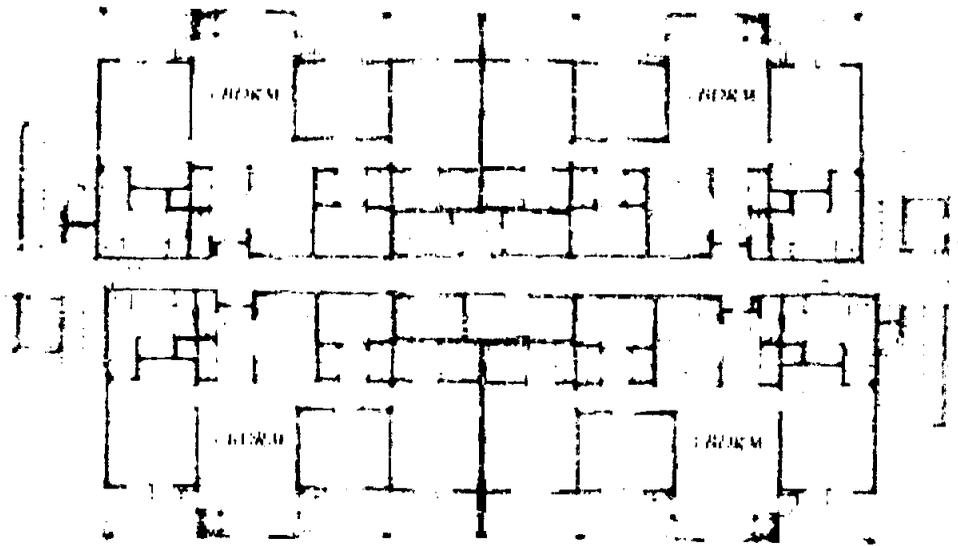
Transportation, Infrastructure and Communications

1. Bike racks;
2. Storm water retention facilities; and
3. Transit stop shelters.

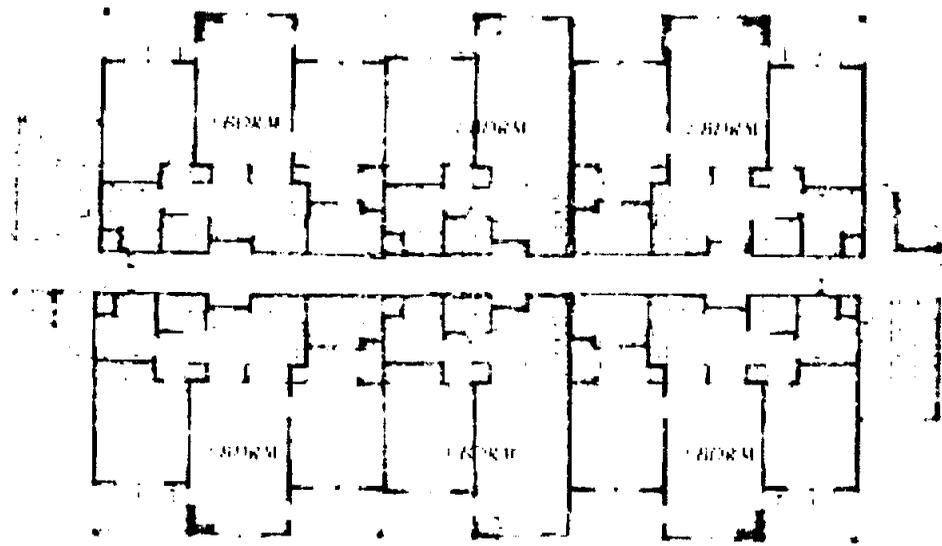


Employee Housing Elevation

EMPLOYEE HOUSING FLOOR PLANS



Ground Level



First Level

Allows unit sizes of:
1 Bedroom: 590 sq. ft.
2 Bedrooms: 903 sq. ft.
3 Bedrooms: 1,043 sq ft.

TEMPORARY PERMITTED USES – Attached Residential

1. Commercial filming;
2. Model units;
3. Offices, temporary real estate;
4. Temporary construction trailers;
5. On-site contractors construction yards; and
6. Similar temporary uses which, in the opinion of the Community Development Director are compatible with the subject land use surrounding land uses and can be conditioned sufficiently to ensure their temporary nature and term.

GENERAL DEVELOPMENT STANDARDS – Attached Residential

- | | |
|----------------------------|---|
| 1. Minimum Lot Area | See Tentative Map. |
| 2. Between Structures | As required by the Town Building Code. |
| 3. Floor Area Ratio | No maximum. |
| 4. Maximum Building Height | 35 feet or 3½ stories whichever is less (as measured from top of roof pitch to midpoint of finished grade and does not include chimney height). |
| 5. Parking | See Programs, Chapter IV. |

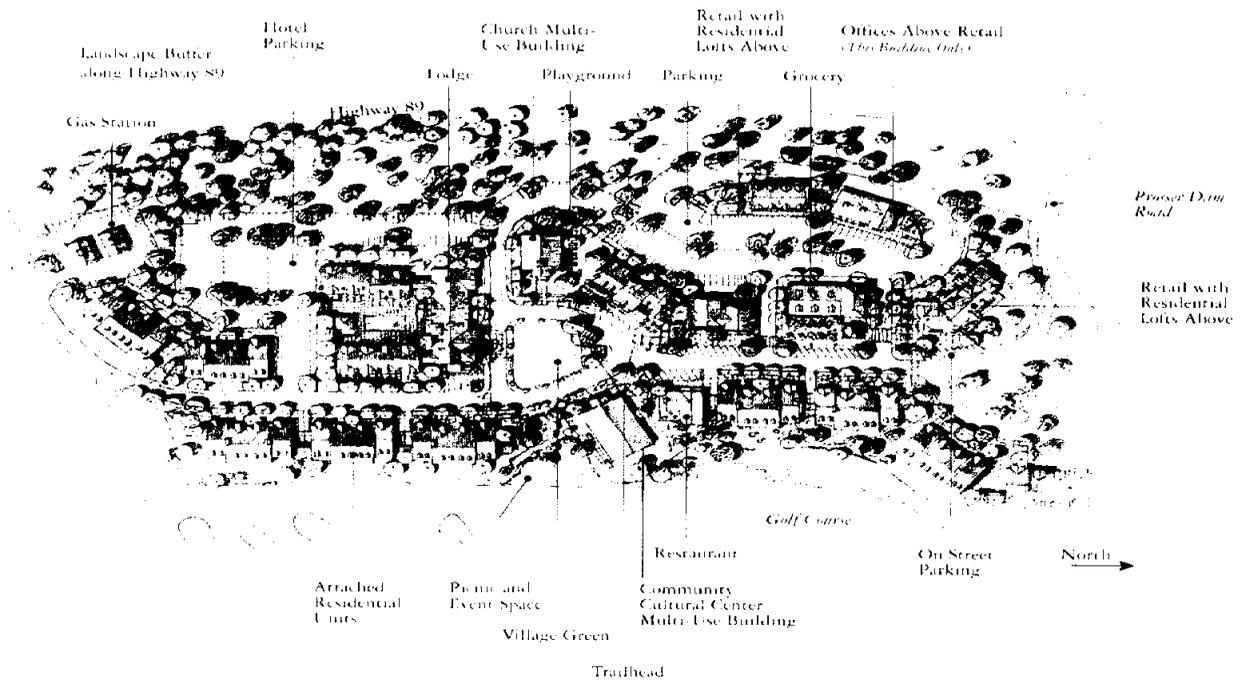
These setbacks and standards may be adjusted to avoid tree loss or site alternations in accordance with the requirements of the Town's Development Code.

VILLAGE CENTER DISTRICT

The Specific Plan District for a mixed-use Village Center is intended to provide land area for a range of retail, office, lodging services and business activity relating to the needs of the recreational activities and surrounding neighborhoods. The project proposes lodging units, community or church facilities, and residential loft units above the retail shops.

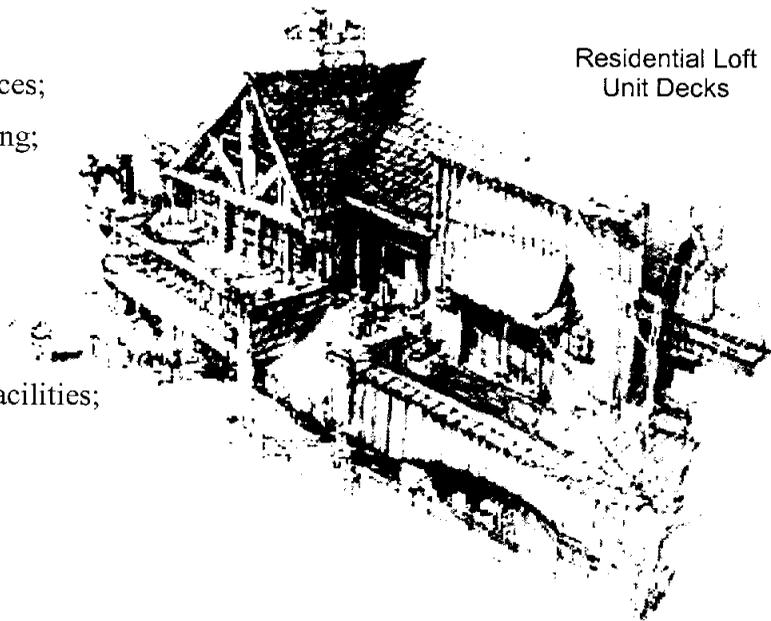
The permitted, conditionally permitted, and temporary permitted land uses and development standards are based on the provisions set forth in the Town of Truckee Development Code, Chapter 18.08, Residential Zoning Districts. To reflect the design standards and use locations required by the Gray's Crossing Specific Plan, the uses listed here are more restrictive than those permitted in the Development Code.





PERMITTED USES – Village Center

1. Banks and financial services;
2. Bars and drinking establishments;
3. Bed and Breakfast Inns;
4. Business and professional offices;
5. Caretaker and employee housing;
6. Child day care centers;
7. Churches;
8. Community centers;
9. Convenience stores;
10. Cultural and performing arts facilities;
11. Gas stations/car washes;
12. Grocery stores;
13. Health and fitness center;
14. Hotels, motels and lodges;
15. Live/work quarters;
16. Membership organization facilities;
17. Outdoor commercial recreation;





Gas Station

PERMITTED USES – Village Center *(continued)*

18. Outdoor retail sales and displays;
19. Parks and playgrounds;
20. Post offices;
21. Public buildings and structures;
22. Public safety and utility facilities;
23. Restaurants and cocktail lounges. including outdoor dining with table service;
24. Retail stores, general merchandise;
25. Retail uses;
26. Theaters and meeting halls;
27. Timeshare units;
28. Urban parks/plazas; and
29. Visitor center.

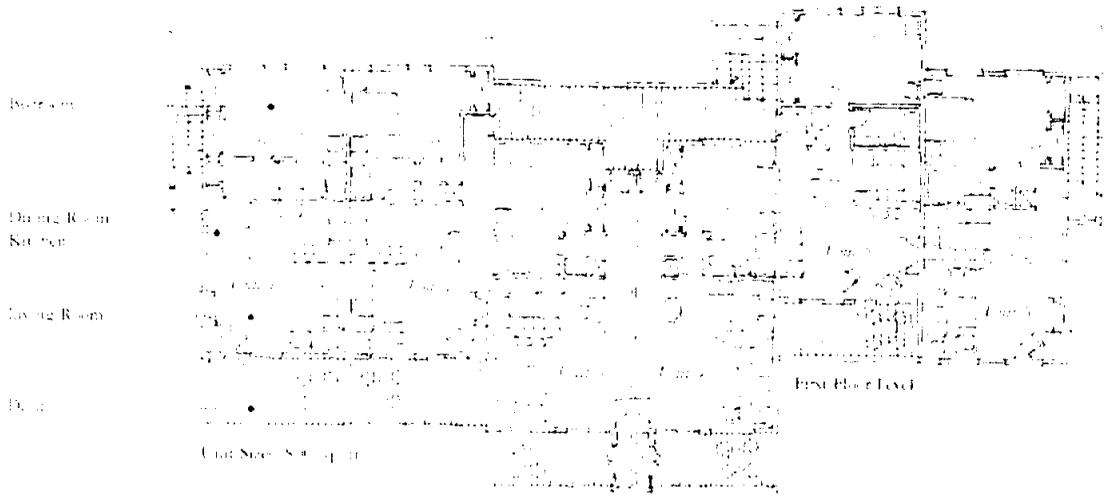
Upon approval of both the Community Development Director and the Planning Commission, other uses similar to those listed above or uses that compliment the above may be considered under the provision of a Conditional Use Permit.

Transportation, Infrastructure and Communications Uses

1. Bike racks;
2. Storm water retention facilities;
3. Transit stop shelters; and
4. Utility lines.



Residential Loft Units



TEMPORARY PERMITTED USES – Village Center

1. Commercial filming;
2. Offices, temporary;
3. Outdoor retail sales, temporary;
4. Seasonal sales lot;
5. Temporary events;
6. Temporary work trailers (office and related uses associated with active construction projects);
7. Temporary outdoor display and sales of merchandise within commercial land uses, including only merchandise customarily sold on the premises by a permanently established business; and
8. Similar temporary uses which, in the opinion of the Community Development Planning Director are compatible with the subject land use surrounding land uses and can be conditioned sufficiently to ensure their temporary nature and term.

GENERAL DEVELOPMENT STANDARDS – Village Center

- | | |
|----------------------------|---|
| 1. Minimum Parcel Area | No minimum. |
| 2. Maximum Building Height | 50 feet or 3 ½ stories whichever is less. |

Churches, community centers and quasi-public uses shall not be considered in maximum floor area allowed in the Village Center District.

These setbacks and standards may be adjusted to avoid tree loss or site alternations in accordance with the requirements of the Town's Development Code.

RECREATION DISTRICT

The Recreation District is intended to provide tourist oriented land uses ranging from country club and restaurant, health and fitness center, 18-hole golf course, driving range and golf maintenance facilities, to a middle school with playgrounds and pedestrian/bike trails.

The permitted, conditionally permitted, and temporary permitted land uses and development standards are based on the provisions set forth in the Town of Truckee Development Code, Chapter 18.16, Special Purpose Zoning Districts. To reflect the design standards and use locations required by the Gray's Crossing Specific Plan, the uses listed here are more restrictive than those permitted in the Development Code.



Clubhouse

PERMITTED USES – Recreation

1. Caretaker housing;
2. Live/work quarters;
3. Nature reserves;
4. Offices, accessory to primary use;
5. Ponds and golf irrigation ponds; and
6. Trails, bike and hiking.

Recreation, Education and Public Assembly Uses

1. Conference facilities;
2. Golf course and country clubs;

PERMITTED USES – Recreation *(continued)*

3. Golf driving range;
4. Golf maintenance facilities;
5. Health and fitness center;
6. Ice skating rink;
7. Outdoor commercial recreation;
8. Outdoor lighted recreational facilities;
9. Parks and playgrounds; and
10. Private recreational facilities.
11. Upon approval of both the Community Development Director and Planning Commission other uses similar to those listed above or uses that compliment the above may be considered under the provision of a Conditional Use Permit.

Retail Trade Uses

1. Accessory retail uses;
2. Proshop with retail sales; and
3. Restaurants and cocktail lounges, including outdoor dining with table service.

Service Uses

1. Public safety and utility facilities.

Transportation, Infrastructure and Communications Uses

1. Bike racks;
2. Electric utility facilities;
3. Pipelines;
4. Telecommunications facilities; and
5. Transit stop shelters.

TEMPORARY PERMITTED USES – Recreation

1. Commercial filming;
2. Temporary outdoor display and sales of merchandise within commercial land uses, including only merchandise customarily sold on the premises by a permanently established business;
3. Contractors construction yards; and
4. Similar temporary uses that, in the opinion of the Community Development Director, are compatible with the subject land use and surrounding land uses and can be conditioned sufficiently to ensure their temporary nature and term.

GENERAL DEVELOPMENT STANDARDS – Recreation

- | | |
|------------------------|---------------------------|
| 1. Height Limit | 24 feet or 2 stories. |
| 2. Parking and Loading | See Programs, Chapter IV. |
| 3. Signs | See Programs, Chapter IV. |

These setbacks and standards may be adjusted to avoid tree loss or site alternations in accordance with the requirements of the Town's Development Code.

OPEN SPACE DISTRICT

The Open Space District is intended to provide land area devoted to maintaining the scenic beauty and natural characteristics of the area. The Open Space District is also intended to provide for public and private recreational uses provided that integrity of the areas natural characteristics is maintained. Lands within Open Space Districts will be separated from other Districts by dedication to Land Trust, conservation easements, zoning restrictions and or other programs that will ensure both preservation and maintenance of the property.

The Gray's Crossing Specific Plan recognizes different types of Open Space. They are: (1) Open Space set aside in large parcels which can be maintained by Property Owners' Associations, transferred to Land Trusts or public entities, and/or reserved by conservation easements; and (2) Open space on private ownerships which is restricted from development, such as rear yard setbacks in the Residential District, open space buffers in the Village Center and Attached Residential Districts, and lands within the Golf Parcels not devoted to fairway construction.

For the purposes of calculating open space for compliance with General Plan Table 2.2 for the PC-2 land use designation, the Development Area Map calculates undeveloped land within the Golf and Open Space Parcels. Open space within the Residential, Attached Residential and Village Center Districts are not included in the open space calculations. Total open space area, excluding the Village Attached and Residential areas, equates to approximately 417 acres.



The permitted, conditionally permitted, and temporary permitted land uses and development standards are based on the provisions set forth in the Town of Truckee Development Code, Chapter 18.16, Special Purpose Zoning Districts. To reflect the design standards and use locations required by the Gray's Crossing Specific Plan, the uses listed here are more restrictive than those permitted in the Development Code.

PERMITTED USES – Open Space

Agricultural, Resource and Open Space Uses

1. Agricultural accessory structures;
2. Buffer zones;
3. Community facilities;
4. Forest management practices;
5. Landscape corridors;
6. Nature reserves;
7. Passive recreation;
8. Picnic areas;
9. Trails, biking and hiking; and
10. Water features.

Transportation, Infrastructure and Communications Uses

1. Bike racks;
2. Electric utility facilities;
3. Pipelines;
4. Storm water detention/retention;
5. Telecommunications facilities; and
6. Utility lines sewer water and drainage facilities.

TEMPORARY PERMITTED USES – Open Space

Temporary uses which, in the opinion of the Community Development Director, are compatible with the subject land use surrounding land uses and can be conditioned sufficiently to ensure their temporary nature and term.

Design Guidelines

CHAPTER III DESIGN GUIDELINES

DEVELOPMENT SUBJECT TO DESIGN REVIEW

All development located within the Specific Plan area is subject to the design standards contained herein and the Design Guidelines set forth in Chapter 18.24 of the Truckee Development Code. The Owners' Association shall review the proposed Development Permit for conformity with Design Guidelines of the Specific Plan and the Truckee Development Code. In reviewing the design of the proposed development, emphasis shall be placed on the following elements that, when combined, meet the design theme of this Specific Plan.

1. Exterior Building Alterations
2. Fences and Walls
3. Grading and Drainage (see Truckee Grading Ordinance for specific standards)
4. Landscaping
5. Lighting
6. Painting
7. Parking and Circulation Areas
8. Signage

Once the Owners' Association has approved the design concepts set forth within these guidelines, application for Design Review and Development Permit, per Article 4 of the Truckee Development code, shall be submitted to the Community Development Department. Permit and approval discretion for all development permits is limited to conformance and compliance with the provisions of the Gray's Crossing Specific Plan, the Gray's Crossing Conditional Approval (Resolution No. 2003-22) and the Gray's Crossing Development Agreement (Ordinance Nos. 203-07 and 203-08).

For clarity purposes, these Design Guidelines are segregated into two sections: one for landscape and one for building.

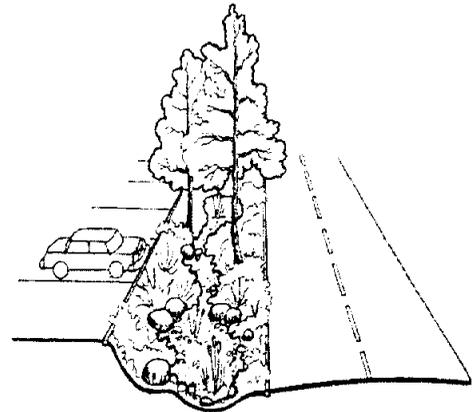
LANDSCAPE DESIGN

GENERAL GUIDELINES

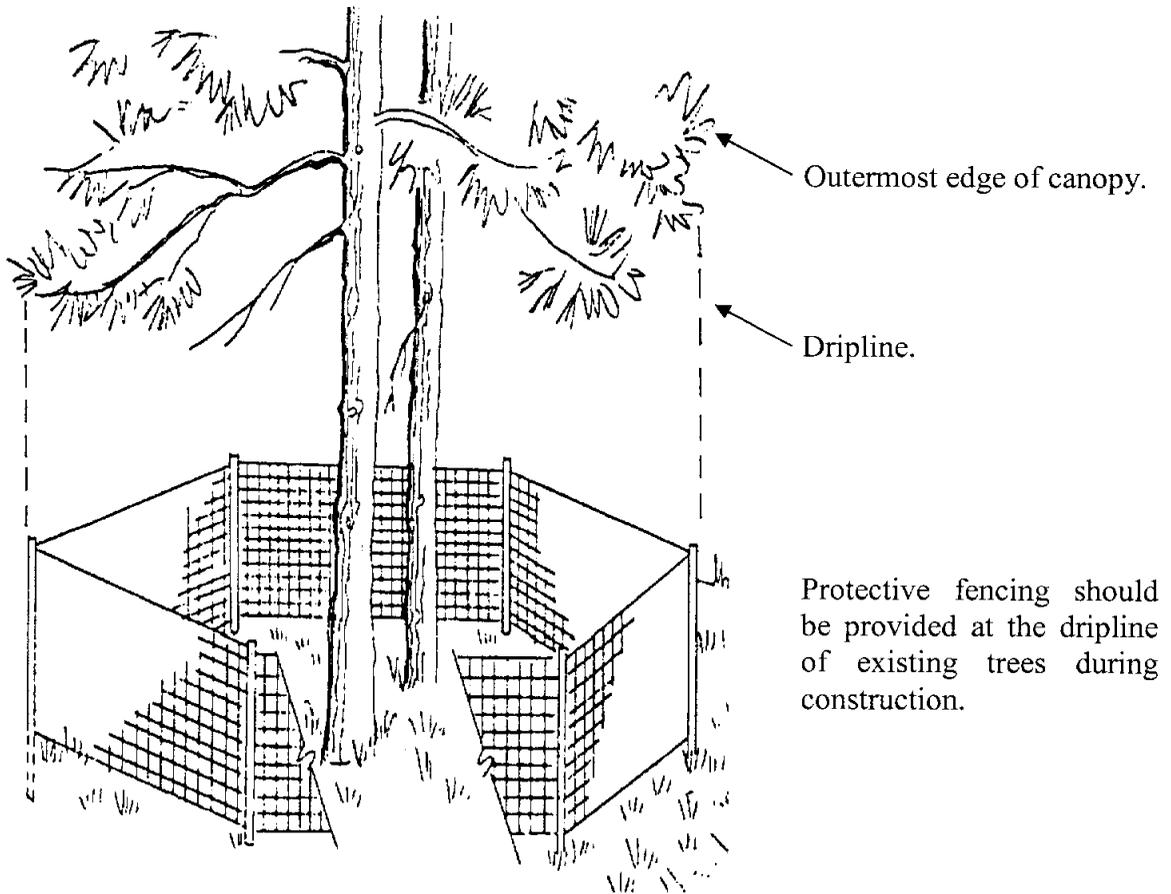
The general guidelines listed below are applicable to all land use designations throughout the Specific Plan area.

1. Landscaping should be used to soften, frame and enhance the visual quality of the development.
2. Landscaping should function to enhance land use and user comfort. These functions may include wind deflection, moderation of heat and glare, muffling noise and reducing soil erosion.
3. Landscaping should be in scale with adjacent structures and be of appropriate size and maturity to accomplish its intended purpose.
4. Landscaping should incorporate multi-layering of plant materials by including trees and shrubs, in addition to grass or ground cover.
5. Vines and climbing plants integrated upon buildings, trellises and perimeter walls are strongly encouraged on otherwise undetailed walls or surfaces.
6. All plant materials should be spaced so that they do not interfere with adequate lighting or restrict access to emergency apparatus such as fire hydrants or fire alarm boxes. Trees should not be located within 10 feet from a fire hydrant, light standard or power pole.
7. Landscaping should allow adequate sight distance for motorists, particularly at neighborhood and project entries.
8. Areas for onsite retention of water should emulate natural forms such as ponds or streams. These features should incorporate the use of earth berming, native rock or boulders and indigenous vegetation.

Areas for on-site retention of run-off should emphasize natural forms and incorporate boulders and indigenous vegetation.



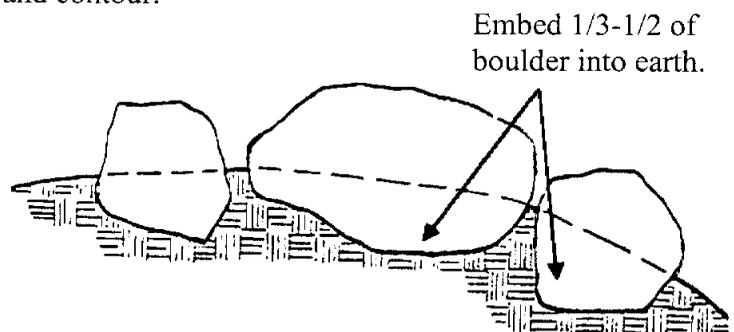
9. All efforts shall be made to incorporate existing trees, other vegetation and natural features into the fabric of the landscape setting. Before final site planning is completed, areas of existing vegetation shall be located for incorporation into the final site plan where practicality and grading allow for preservation. All vegetation to remain shall be fenced with temporary fencing at the drip line of the tree prior to the beginning of construction (the drip line is the radius from the trunk required for water percolation and gas exchange).



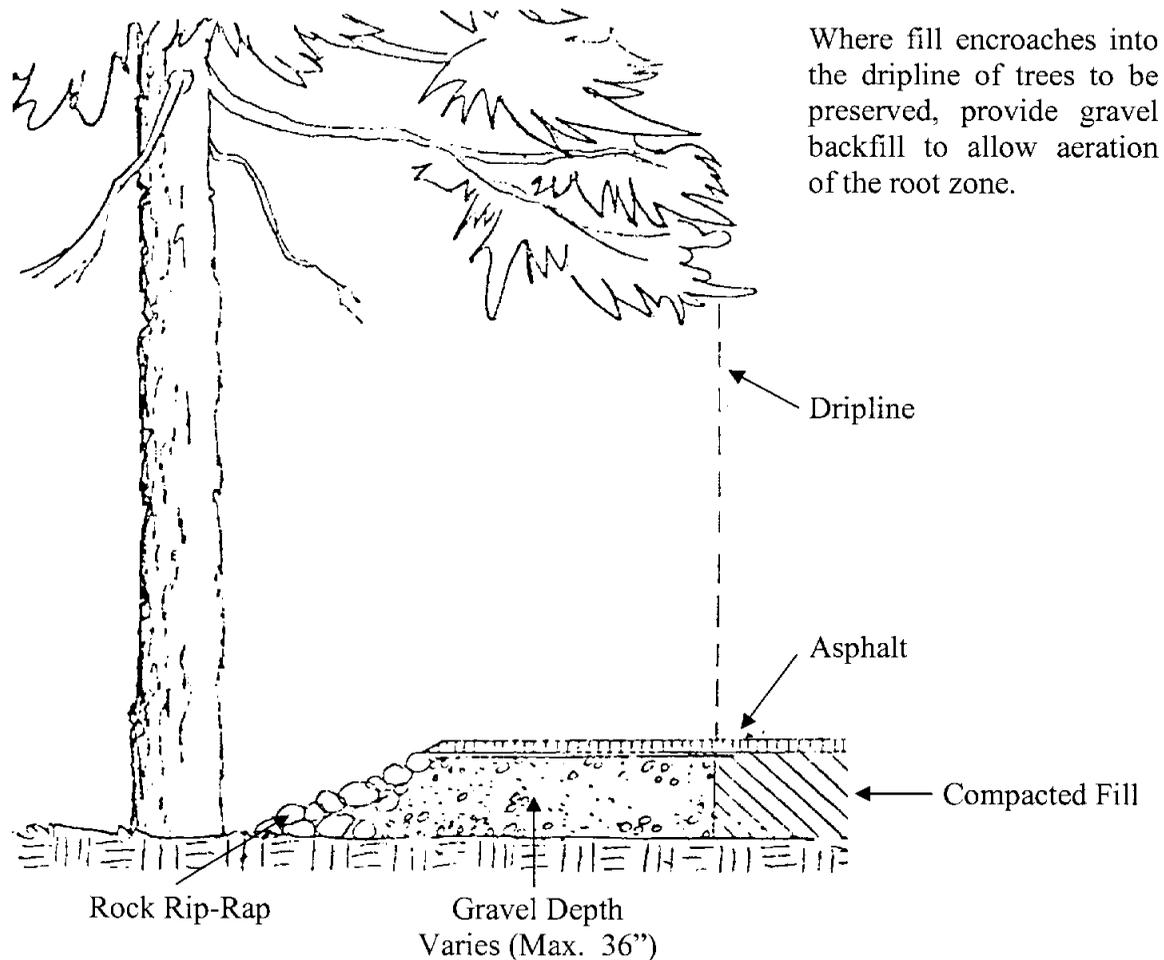
10. Graded areas should have large landscape islands. Large native boulders should be used in these islands to add variation and contour.

If possible, use native boulders with lichen, moss and unusual features.

Large native boulders should be included in the landscape wherever possible.



11. Where necessary, structures and grading shall be allowed to encroach into the drip line on one side only. Rock riprap may be used to keep areas of fill away from the trunks of trees.



12. The Xeriscape concept shall be used in the landscaping areas and plants of similar water use should be grouped to reduce irrigation needs.
13. Soil amendments and surface mulching of landscape areas shall be provided to increase water retention capacity of native soil.

PLANT SELECTION

GENERAL GUIDELINES

1. For recommended plant type and standards, see Chapter 18.42 of the Truckee Development Code.
2. Native and drought tolerant/water conserving plant materials shall be encouraged within the Specific Plan area. These types of plants offer color, texture and character indicative of the 6,000-foot elevation level of the Sierra Nevada Mountain Range. If non-native plant material is used, it should be selected for its visual and cultural compatibility with the existing environment.
3. The lodgepole pine (*Pinus contorta*), Jeffrey pine (*Pinus jeffreyi*), ponderosa pine (*Pinus ponderosa*), mountain alder (*Alnus tenuifolia*) and quaking Aspen (*Populus tremuloides*) shall be the predominant landscape element within the plant palette of the Gray's Crossing Specific Plan. These trees are valued for their unique structure and attractive foliage and are native to the region. These trees adapt to the climate and soils of the area and will play a major role in the establishment of an appropriate landscape for the project area.
4. The aspen, alder, and pines should be integrated within the overall network of streetscape planting and neighborhood landscapes as a unifying element.

LANDSCAPE MAINTENANCE

PURPOSE

These guidelines are intended to provide for regular maintenance of landscaping including planting, pruning, irrigating, plant replacement and other maintenance requirements for the health and quality appearance of landscaping within the Specific Plan area.

GENERAL GUIDELINES

1. All planting areas shall be maintained in a neat, clean and healthy condition. This shall include proper pruning, mowing, weeding, removal of litter, fertilizing, replacement of plants when necessary.
2. Walkways shall be kept clear of debris from maintenance operations, erosion runoff from storms and irrigation and windblown debris.
3. A permanent, automatic underground irrigation system shall be installed and programmed to deliver adequate moisture as determined through personal inspection by a qualified professional.
4. All irrigation systems shall be maintained in good working order and protected from freezing.
5. Soil moisture that promotes vigorous growth of all plant material shall be maintained.
6. Cleaning and adjustment to irrigation systems shall take place on a regular basis, as part of regular maintenance activities.
7. All landscape catch basins, swales, channels and other drainage devices should be maintained in a state conducive to conducting water in a free flowing manner.
8. Maintenance procedures should not rely heavily on the use of herbicides, pesticides or fungicides. Proper plant selection and maintenance will alleviate the need for frequent use of chemicals.

LANDSCAPE ZONE GUIDELINES AND DESIGN THEMES

PURPOSE

Landscape concepts have been organized into landscape “zones” according to their hierarchy as visual elements, specific design and implementation characteristics. The landscape concept for each zone consists of recommended design techniques and minimum standards for landscape implementation.

ZONE 1 – VILLAGE CENTER

The Village Center District is intended to provide office/retail lands for public use. A major element in developing land within this designation is to incorporate the sites natural features in the site planning design and to provide landscaping that complements the architectural elements of the design guidelines. This area needs a strong first impression and defining element. Therefore, a unique streetscape treatment using native plants along the roadways is the recommended theme for this area.

Establishing a visual character that borrows from the region’s heritage and climatic conditions is the primary purpose of the landscape design theme for this Zone. (For plant type and standards, see Chapter 18.42 of the Truckee Development Code.)

Landscape site plans in this zone should be developed to highlight stands of large native Jeffrey Pines. These large pines will provide visual relief of the larger building masses anticipated in this zone and will break up the expanses of necessary parking. These large islands will also serve as additional snow storage for parking areas. In this way, snow storage can be avoided in areas of immature, fragile planting. Groups of deciduous trees, such as Aspen, should be used to supplement native trees.

Shrub masses and ground cover would be included to screen parking areas from view, soften buildings and provide seasonal interest. Limited areas of perennial color could be used as accents near signage or in areas where pedestrian traffic is restricted. Trash enclosures and recycling areas should be screened with a combination of masonry walls and shrubs. High pedestrian use in these areas should be accommodated with wide, convenient walkways and pedestrian furnishings such as benches and trash receptacles. Planting adjacent to buildings should be able to withstand potential snow shed and runoff from roofs.

The streetscape landscaping should emphasize a predominance of Jeffrey Pine, Lodgepole Pine, Aspen and Mountain Alder. These should be used in informal groupings along the street and in groupings or groves in courtyards or planter areas. Street trees in planter areas will provide separation of the street from the pedestrian walkways. The overall planting plan for the boulevard should provide a strong sense of identity for the project while creating a pleasant experience of passage, rhythm and seasonal interest for this area.

ZONE 2 – RECREATION

The Recreation District is intended to support the golf course, practice range, maintenance facility, clubhouse, proshop, fitness center, restaurant and lounge facility. Emphasis on design is needed to create a recreational atmosphere.

Planting Design Technique

The plant palette for this zone includes a rich variety of native and ornamental plant species. Entry areas and focal points are punctuated with masses of perennial color to enhance the recreational image during the spring, summer and autumn months. Areas of turf should provide for specific limited recreational opportunities where appropriate. Ornamental trees and shrub planting should define use areas, complement building architecture and provide seasonal interest.

Golf Course

The 18-hole golf course will cover approximately 109 acres. Design and maintenance plans are intended to be compatible with the area's natural resources, preserving open space and fostering an ecologically sound environment through water quality protection and Best Management Practices (BMPs) as defined in the Natural Resources Management Plan (NRMP).

Landscaping

The general landscape concept for Gray's Crossing is to fine grade each of the fairways leaving as many of the trees and other natural vegetation as possible. Other than the turfed area of the golf course, the only areas to receive landscaping would be those disturbed by grading and not planted to turf. The plant types and maintenance shall conform to the provisions set forth in the NRMP.

Cart Paths

Cart paths will be used to convey traffic around each green, to the following tee and along the fairways. In areas where cart paths are to be employed, they will be eight (8) feet wide with an asphalt-concrete surface. Runoff from cart paths and other impervious portions of the course will be retained in on-site storm water management areas (BMPs) that utilize natural processes to remove pollutants and encourage infiltration of runoff. Retention BMPs will be sized to meet the California Regional Water Quality Control Board, Lahontan Region standards for the 20-year, 1-hour storm.

Irrigation

Irrigation water and water features will be obtained primarily from on-site domestic potable water from the Truckee Donner Public Utilities District.

The timing of irrigation will be set to coincide with off-peak electrical rates beginning at 10:00 p.m. and ending at 6:00 a.m. (8 hours). The irrigation will be sized to allow for the full irrigation of the facility within an 8-hour window to accommodate time changes and sprinkler inefficiencies.

Improved Rough

This vegetation will consist of native vegetation, such as grasses and wildflowers that will occupy portions of the rough that are expected to receive heavy traffic. Native sagebrush/bitterbrush, which occupies most of the site presently, cannot tolerate golfers walking over it, searching for lost balls, hitting balls from it, or the assorted other disturbances common along golf courses. In more heavily used portions of the rough these sorts of disturbances will quickly result in broken and dying shrubs surrounded by bare ground, which will be aesthetically unappealing and will represent a potential erosion problem.

Improved rough will be designed to tolerate the types of disturbances present along fairway edges, while requiring little maintenance, providing habitat for wildlife and remaining attractive throughout the year. The improved rough will be an area from which it will be difficult to hit a golf ball cleanly. In this case, improved rough will be similar to the undisturbed native rough in terms of golf playability, but will be superior to native rough in terms of its ability to withstand disturbance. The vegetation will allow golf balls to be hit somewhat more easily than is possible in native sagebrush, but with more difficulty than is typical of turf grass rough.

Occasional mowing and some irrigation of the improved rough will be needed to keep the plants growing throughout the summer and increase their ability to recover from divots and other disturbances.

Fertilizers

Nutrients used will be nitrogen, potassium and phosphorus. Soil nutrition and fertilization practices at Gray's Crossing will be designed to protect the environment and water quality by using accurate application rates and timing by:

1. limiting irrigation to the replacement of soil moisture;
2. using slow-release fertilizers; and
3. timing fertilizer application and concentration with active plant growth, seasonal root activity, photosynthesis and seasonal rainfall.

Gray's Crossing will use a state-of-the-art golf course irrigation system which includes an on-site weather station and a computerized fertilizer injection system designed to supply plant nutrients in ultra low amounts at prescribed intervals. This process, known as "fertigation", delivers nutrient irrigation water at micro-managed water soluble foliar rates and will reduce the amount of fertilizer concentration in the environment and the potential for traditional granular fertilizer application to impact ground and surface water quality. Furthermore, this will minimize the use of fertilizers and turfgrass chemicals on the golf course. The objective is to match the release of the nutrient with the uptake of the plant.

Natural Resources Management Plan (NRMP)

As part of the water quality mitigation measures being implemented, Gray's Crossing intends to adopt an NRMP and golf course management plan similar to those outlined in Nevada County's CHAMP guidelines and in the Audubon International program.

The purpose of a NRMP is to outline general management requirements and guidelines which are applicable for the use of chemicals during operation and maintenance of the golf course and is intended to be used after the construction of the course. The construction of the golf course will follow all regulatory requirements in conjunction with the National Pollution Discharge Elimination System (NPDES) permit process for construction and will follow appropriate Best Management Practices (BMPs) as required by law. The incorporation of a NRMP is voluntary. The NRMP developed for Gray's Crossing will be responsive to project and site specific needs following these guidelines:

1. Use only chemicals and chemical application techniques which are lawfully permitted in the State of California. No chemicals will be used on the golf course that have been prohibited from use within the State as reflected on the Proposition 65 list.
2. Avoid chemical applications in unpredictable weather when a sudden storm may occur immediately following an application.
3. Act responsibly when making applications and follow label directions.
4. Know the key chemical properties of the chemicals that are being applied.
5. Follow chemical storage and disposal techniques recommended by the manufacturer specification.
6. Comply with Chapter XI of Land Use and Development Code of California Department of Environmental Health.

The intent of the Gray's Crossing golf course is to be a "green" course by implementing organic farming techniques to support the growth medium so the soil will, in turn, support the turf. The NRMP will be prepared and submitted to the Town of Truckee, Nevada County, the California Regional Water Quality Control Board, Lahontan Region for approval.

The Lahontan Basin Plan has water quality objectives for pesticides which include insecticides, herbicides, rodenticides, fungicides pesticides and all other economic poisons. If these chemicals are used, they would only be utilized in accordance with State Law, and it is stressed that these spot applications would be limited and controlled. Monitoring would be conducted to assure that no chemical is migrating past its use area. An ongoing monitoring program as set forth in the NRMP and Storm Water Pollution Prevention Plan (SWPPP) will provide long-term monitoring to ensure that the goals of the NRMP are being met.

ZONE 3 – ATTACHED RESIDENTIAL

This District is intended to provide various types of housing opportunities ranging from “affordable” to townhomes. Frontages on the golf course fairways and close proximity to the Village Center make this area a unique design challenge. This blend of housing near the office/retail and recreation areas makes this area capable of village status. Emphasis on buffer yards, formal planting yards and pedestrian friendly streetscapes are key elements of this multi-density setting.

Landscape planting in this zone should enhance the identity of the various Attached Residential units. Landscape screening with trees and shrubs for privacy is of primary importance. Smaller scale people-friendly spaces, such as courtyards or pocket parks, create a sense of neighborhoods within the larger framework of the Attached Residential District. Turf areas should be provided for passive, family oriented recreation. Durable planting of shrubs, trees and perennial color will define spaces, create a sense of identity and provide seasonal interest. This buffer should provide a natural landscape theme of existing trees, additional trees planted in groves, undulating earth berms, and boulder clusters. Occasional areas of perennial color are included to add detail and interest.

ZONE 4 – RESIDENTIAL – Single-Family

The Residential District (single-family) is intended to provide low-density residential housing opportunities. Emphasis is placed on maintaining and preserving existing tree cover and vegetation. A variety of conservation techniques are recommended to preserve existing vegetation and maintain natural ground contours.

Introduction of plant species should be restricted to formal planting areas, such as driveway entrances, front and backyard gardens areas and streetscapes.

Private homeowner landscaping should emphasize the incorporation of existing trees or other natural features, such as rocks and topography. Additional planting should be of native and adaptive species with very limited areas of turf or ornamental planting. In this way, the residential areas will blend in color and texture with the surrounding environment. Graded areas should be protected from erosion and re-vegetated with native species. Large native boulders may be used for stabilization of cut slopes at roadway or building pads.

ZONE 5 – RESIDENTIAL – Cottage Housing

The landscaping theme for this Residential District (cottage housing) is two-fold: streetscape and courtyards. The streetscape for the cottage units is utilitarian in design. The intent is to provide garages and guest parking off of dead-end streets with access to the units.

The front of each unit will be facing a landscaped courtyard primarily designed to be maintained in common with adjacent property owners and serve as open space, passive recreation and some active recreational areas. Landscaping should be designed around picnic/barbecue areas that are natural in their efforts to maintain existing trees and vegetation with some introduction of boulders and seasonal color to provide variation. Each cottage unit will have a small privacy

area that can be landscaped individually. However, the main emphasis is on a centralized landscaping/courtyard theme maintained by the property owners' association to keep a consistent theme and maintain quality.

ZONE 6 – OPEN SPACE

The Open Space land use designation is intended to provide for and promote the natural character of the area. The Specific Plan devotes 417 acres of land to Open Space. A planned trail network allows hiking and biking trails that connect the various development pods. Open Space land will primarily be undisturbed except for general maintenance and “fire safe” forestry practices.

DESIGN CRITERIA / GUIDELINES

RESIDENTIAL DISTRICT – Single-Family/Cottage Homes

Purpose

These Design Guidelines shall apply to all single-family and cottage homes constructed in the Residential District. The purpose of these guidelines is to create indigenous mountain architecture in quality residential subdivision developments.

General Guidelines

General

Preservation of the natural environment, while achieving a harmonious relationship with neighboring man-made structures, is the foremost objective of the design review process. Each structure design in Gray's Crossing must be compatible with the natural terrain. Building forms should relate to natural surroundings, but buildings in close proximity to one another must also have compatible design qualities.

Although some owners may be competent in planning and designing their own home, all owners are encouraged to seek professional design assistance in the planning and design of their homes, additions or revisions, including landscaping and color changes. Professionals familiar with the mountain environment can provide the owner with an understanding of the site possibilities and assist the owner in realizing the special qualities desirable for this area. In addition, the professional will be able to present the submittals required for development review.

The existing natural features of a site should be retained and incorporated into the designs of all improvements. The integration of features such as creeks, trees, natural slope, rocks and views often leads to creating a "sense of place" and blends buildings and improvements with the site.

Buildings should be sited so that they do not interrupt the flow of the skyline as viewed from common vantage points.

Site plans should be compatible with adjacent properties and streetscape in the placement of structures and uses.

Buildings should be sited with consideration given to sun and shade, changing climatic conditions, noise, safety and privacy. Buildings should be located on a site so as to enhance the natural features of the site. In general, off-street parking should be located to the sides and rear of the site and screened from the street with landscaping, in such a way that it will not cause problems regarding snow removal or sight distance.

Buildings and spaces should have a strong functional relationship to the site. Required side and rear yards, open space and snow storage should be utilized and integrated into the overall site arrangement. Leftover spaces and inaccessible yards do not permit full utilization of the site.

RESIDENTIAL DISTRICT – Single-Family/Cottage Homes *(continued)*

Building Design Guidelines

Building design should complement and harmonize with neighboring buildings. Design compatibility can be achieved through similarity of form, height, roof shapes, scale, materials, color or pattern of openings.

Building design should be consistent with all elevations in regards to color, materials, form and detailing in order to achieve design harmony and integrity. Elevations need not look alike for a sense of overall architectural continuity to be present.

Major building forms should express a simplicity and directness responsive to the tradition of mountain architecture. Complexity and contradiction of form and expression should be avoided.

Residential buildings should be in scale with their site, the immediate surroundings and with the area. A large building can be reduced in bulk by breaking it down into component parts that reflect the scale of adjacent buildings.

Access

The places where a driver enters or leaves a site affect both the project and the community as a whole. Care must be taken in locating access to a site in order to avoid creating traffic obstructions or hazards where drivers are entering or leaving a site.

Access points should be kept to a minimum. The number of entrances should be adequate to allow efficient traffic flow. Adequate sight distance should be provided at exits for drivers reentering traffic.

Parking

Off-street parking is required for all residences. Each site is expected to accommodate its parking needs. Parking should be safe and accessible, with a simple layout that is readily understood by the driver. It should be designed and located so that it does not dominate the residence.

Parking areas should be easily accessed from the street. Locating parking to the rear or side of the building is preferred, with the front setback used to create a landscape buffer and snow storage area between the building and street. All maneuvering shall be accomplished on-site. Backing out onto a street is not allowed.

Landscaping of parking areas serves a number of important functions. Perimeter landscaping increases the attractiveness of the site and street by screening the cars. Perimeter planting acts as a visual and noise buffer for adjacent properties.

RESIDENTIAL DISTRICT – Single-Family/Cottage Homes *(continued)*

Lighting

Outside lighting increases the operational efficiency of a site, provides a measure of site security and can enhance the aesthetics of the site and architectural qualities of structures. In determining the lighting for a residence, the source, intensity and type of illumination should be appropriate for the lighting needs.

Exterior lighting should be designed as part of the architectural and site design of a project. Fixture style and location should be compatible with the building's architecture and landscaping.

All site lighting must be low-level illumination. Outdoor lighting on all lots is to be shielded so as not to create light spill or glare.

Fixture mounting height should be appropriate to the use, the residence and the setting. Light standards should be as short as possible. Where low-level lighting (less than 5 feet) is used, fixtures should be placed and directed so as to prevent glare. Where there may be a chance of breakage, shatterproof coverings should be used on low-level lights.

Overall lighting levels should be compatible with the neighborhood ambient light level. The lighting system should be energy efficient, based on the amount of light actually needed by users and viewers. A few well-placed, low-intensity lights can provide all the illumination needed for visibility, safety and security.

Parking area and building lights should be directed downward to prevent spillover onto neighboring properties and streets. Light sources (bulbs) should be concealed.

See also "Lighting" in Programs (Chapter IV).

Materials

Changes in wall material can lend visual interest to a building; too many changes can make the wall visually discordant. The objective should be to create walls that are interesting, but not in competition with their surroundings.

Wall materials should respond to the orientation of the building, with limited window openings on the north and south that are open to sun exposure.

Natural materials such as redwood or cedar board siding, cedar shingles and shakes are particularly appropriate for their natural appearance and weathering qualities. When properly selected and applied, these materials have minimum maintenance requirements.

RESIDENTIAL DISTRICT – Single-Family/Cottage Homes *(continued)*

Roofs

Roof shape is important in terms of organizing the massing of buildings, especially at the edges of trees, landmasses or in the open. The objective in determining roof shape is to establish a visual order to building clusters.

Roof surfacing materials are important as a means of blending the new construction to the existing character of the area, as careful selection of these materials can help to relate the buildings to their surroundings. On the other hand, the wrong color and texture can make the building garish and distracting. From a functional standpoint, the choice of materials depends on the slope and assembly of the roof. The objective is to choose roof-surfacing materials that help the building blend with its site and climatic conditions and which are also functionally appropriate.

Composition shingle roofs and metal roofs are preferred with particular consideration to roof configuration, quality, color and longevity of finish. Roof appurtenances (dormers, clerestories, and skylights) create interesting, pleasant interior spaces. Their location on the roof is critical to avoiding an over-decorated, visually confusing appearance.

Snow diverters and retainers may be necessary installations on roofs. They should be handled as an integral part of the roofscape. Pedestrian and vehicular areas shall be protected from roof snow shedding. This can be accomplished through secondary roofs, snow clips and snow fences on roofs. All roof structures shall be designed to conduct rainwater and snow melt in such a way as to prevent it from creating a dripping, icing or flood danger on pedestrian or vehicular areas below.

For pitched roofs, ice dams should be prevented by either heating the roof overhang or cooling the roof surface with the placement of substantial insulation or unheated spaces under the roof. Pitched roofs should be of an angle and material that will shed snow. Metal roofs and roofs with southern exposures shed snow the easiest.

Eave lines should be high enough to avoid accumulation of snow to the eave edge, where it will prevent snow from sliding off the roof.

Flat roofs should slope to a centrally located drain that runs down through heated space. The guidelines are to provide for the public's safety and convenience and to reduce maintenance costs in heavy snow areas in the County.

Chimneys Vents and Flues

Chimneys and flues shall be designed in such a manner so as not to cause fumigation of ground level areas or adjacent buildings during down slope wind conditions. Chimneys should be located high on the upwind side of the buildings to ensure adequate disbursement.

RESIDENTIAL DISTRICT – Single-Family/Cottage Homes *(continued)*

Chimneys and vents should be located out of the path of sliding snow.

Vents, flues and flashing shall not be exposed galvanized pipe and shall be painted a dark color. Attempts shall be made to group these roof projections and conceal them from public view. This can be done by enclosing them in forms compatible with the structure.

Doors and Windows

Door openings separate two completely different environmental conditions. Door openings should be protected from the wind and overhanging or drifting snow. Vegetation, fences, extended walls, roofs and other features of the building site can help shelter people near the building entries. Where possible, doors should open onto exterior areas that receive sunlight.

Windows may be constructed of wood or wood covered with colorfast vinyl or aluminum. Metal or metal covered windows must be coated with an approved finish.

Building entrances should be covered with a roof and raised to allow for snow build-up. Roof slopes should be designed to avoid the shedding of snow onto steps, entrances and decks and/or paved areas.

Aluminum doors, windows and screens shall be anodized or a factory finished baked enamel in a dark color. Foundations, vents, flashing and other exposed metal shall be painted a dark color. Chimneys, heating vents and utility lines should be located out of the path of sliding snow.

Drainage, Erosion Control and Snow Removal

Site drainage and spring run-off should be carefully considered in the drainage solution. Natural drainage courses should be preserved and existing vegetation (ground cover, shrubs, and trees) should be left undisturbed on the site except where the structures are located and where construction access is necessary.

The site should drain adequately without interfering with adjacent properties. Natural contours should be maintained as much as possible. Buildings, parking areas and drives should be designed to fit the terrain, requiring minimum grading. Cut and fill should be kept to a reasonable minimum. Only those areas needed for construction should be disturbed. Vegetation outside the construction zone should be protected.

All slopes shall be protected against erosion. Grading design cannot create slopes greater than a 2:1 unless controlled by mechanical stabilization, unless otherwise approved. No slope shall exceed the angle of repose for the material involved.

All residences are required to have erosion control plans in accordance with the Landscape Design Guidelines herein.

RESIDENTIAL DISTRICT – Single-Family/Cottage Homes *(continued)*

Site design must address problems of ice hazard and snow removal. Snow storage areas should be provided which are functional in placement and adequate in size. Snow shedding onto adjacent property is not allowed.

Driveway grades should not exceed 16% to allow easy use during icy or snow conditions. Driveways to north-facing garages should slope away from the garage door for a minimum of 20 feet.

Parking areas and driveways should be located to catch afternoon sun in order to speed snow melting and prevent ice build-up. Parking areas should be designed for snow removal operation with unobstructed movement for snowplows.

Other Architectural Guidelines

Fences

Fences are allowed for the purpose of providing screening around hot tubs, pools, garbage containers, utility yards and fuel tanks. No fencing will be permitted along property lines.

Hot Tubs

Hot tubs shall be screened from neighbors' view and placed in such a location so the hot tub facility does not intrude upon a neighbor's privacy.

Skylights

Plastic bubble skylights are not allowed. Flush skylights integrated into the architecture will be considered during Design Review.

Solar Panels

Solar panels shall be integrated into the architectural design and all exposed metal shall be anodized a dark color or factory finish.

Trash Disposal

A structure enclosed on four sides (including doors) with a roof shall be provided to store at least two (2) full-size garbage containers. This structure should be placed in a location that will provide easy access, e.g., along a driveway or walkway and its design shall be compatible with the other major buildings.

RESIDENTIAL DISTRICT – Single-Family/Cottage Homes *(continued)*

Utility and Service Areas

All areas for utility and service activity, including refuse collection areas, must be separately enclosed or screened so as not to be visible from neighboring property.

Utility Lines

All on-site utility and other transmission lines shall be placed underground.

ATTACHED RESIDENTIAL DISTRICT

Purpose

These Design Guidelines shall apply to multi-family homes constructed in the Attached Residential District. The purpose of these guidelines is to achieve an integrated architecture consistent with the overall design of the project while encouraging a variety of design features to avoid monotonous standardization.

General Guidelines

Although flexibility in multi-family residential site planning is desired, the aggregate effect of residential developments being unrelated to one another and the community as a whole often produces isolated “compounds” with little concern for the public environment. Residential developments surrounded by high walls, parking lots and rows of carports along public streets are examples of practices to be avoided.

Building Elevations

Avoid long, unbroken building facades and simple box forms. Building facades should be broken up to give the appearance of a collection of smaller buildings. To the extent possible, each of the units should be individually recognizable. This can be accomplished with the use of balconies, setbacks and projections that help articulate individual dwelling units or collections of units and by the pattern and rhythm of windows and doors.



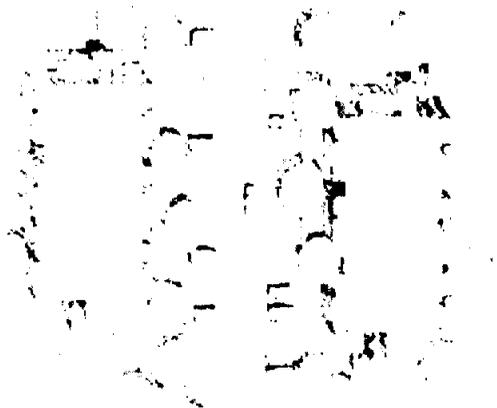
ATTACHED RESIDENTIAL *(continued)*

Open Parking and Carports

Open parking and carports should be clustered in parking courts and along streets. Parking spaces in the front yard setback should not be used to satisfy the off-street parking requirements. Pedestrian and automobile circulation shall be clearly defined. Special paving at parking court entries and landscape nodes between parking stalls are encouraged to soften the streetscape.

The materials, colors and roof forms of detached garages and carports should be consistent in design with the adjacent residential buildings. The integration of carports with patio walls is encouraged to add variety and relief to the streetscape. Prefabricated metal carports are strongly discouraged. Carports in a single unbroken row are discouraged.

Street drainage should be collected in curb and/or gutters. The use of center-swale drainage devices is strongly discouraged. Parking lots may drain to a single concrete swale at the edge of the aisle. Use of rear access and alleys are encouraged.



Aisle width requirements are subject to local fire district approval.

Parking stall length may be reduced by up to two (2) feet where conditions permit the overhang of vehicles into six (6) feet or more of sidewalk or a minimum of seven (7) feet of landscaping.

The number of required parking spaces and percentage of compact car spaces are regulated by the Town of Truckee.

Subterranean Parking

Parking beneath buildings, including subterranean parking, economizes the use of land and increases on-site recreation space. Such parking should be considered in all multi-family developments. No parking is allowed in the front yard. If provided at-grade with the street, parking should be effectively screened from public view on the adjoining streets.



ATTACHED RESIDENTIAL *(continued)*

Garage Doors

Garage doors should appear to be set into the walls rather than flush with the exterior wall. Garage door design should be kept simple, clean and unadorned. They are a major visual element of a home.

Entry Identity

In the Attached Residential District it is appropriate to provide each unit with its own identity and entry. This can be accomplished by staggering and offsetting each separate unit and combing one and two story building forms to separate massing. This will also provide variety to the streetscape.

Landscaping

Refer to the Landscape Design Guidelines herein.

Architectural Design Guidelines

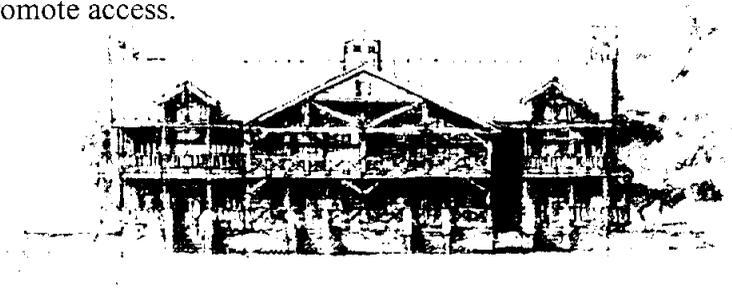
General

There is no particular “style” proposed for Attached Residential structures in Gray’s Crossing but the focus should be on constructing a high quality residential environment, which is response to the climate and the surrounding mountainous landscape. The criteria presented here strive for this “quality” architecture through descriptions of encouraged materials and architectural considerations.

1. Long exterior walls that do not incorporate effects must be avoided on all structures.
2. Architectural planning and design should take full advantage of energy efficiency, e.g., natural heating and/or cooling, sun and wind exposure and solar energy opportunities.
3. Structural form and scale should relate to the use of the building as a multi-family residence. The scale of all buildings should be within a human scale so as not to overwhelm or dominate the natural surroundings.
4. Avoid the use of long access balconies or corridors that are monotonous and impersonal. Instead, access points to units should be clustered. To the extent possible, the entrances to individual units should be plainly visible.
5. Change roof levels and ground planes to break up the mass and bulk of buildings.
6. Make extensive use of private, enclosed patios and balconies to provide residents with a greater degree of control over their living environments.

ATTACHED RESIDENTIAL *(continued)*

7. Provide building complex entrances that are distinctive and easily identifiable. To the degree possible, entrances to individual units or clusters of units should also be distinctive and easily identifiable.
8. Mixed uses in close proximity to needed services are encouraged to minimize vehicle trips and promote access.



Materials

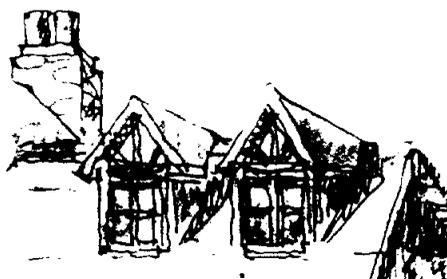
Wood siding has traditionally been the primary wall surface material utilized throughout the Truckee area for residential structures. Particular attention should be given to selecting an appropriate accent material. The following design considerations shall determine if a development meets the general architectural criteria:

1. Stucco, smooth, sand or light lace finish;
2. Wood, as a primary and accent material, such as shingles, board and batten, etc.;
3. Brick, as primary and accent material; and
4. Native rock, as an accent material.

Roofs

While few, if any, roof types for multi-family structures will actually be prohibited by these guidelines, the following types/styles are encouraged and should be considered prior to final selection.

1. Gable, shed and hip roofs;
2. Combining roof types;
3. Creating articulation in ridgeline plane; and
4. Varying plate heights and ridge height.



ATTACHED RESIDENTIAL *(continued)*

Doors and Windows

Main or front doors should be double or single wood doors.

Window openings in mass walls should be relatively small in scale and be used in an informal pattern on the wall, with deep set reveals and varied proportions. The following window types are encouraged in the design of buildings.

1. Bay Windows
2. French Doors
3. Variety of Window Types
4. Rectangular Windows
5. Clerestory Windows



Other Architectural Guidelines

Exterior Stairs

Simple, clean bold projections of stairways are encouraged to complement the architectural massing and form of a building. Stairways should be smooth stucco, plaster or wood with accent trim of complementary colors.

Balconies, Porches and Patios

The incorporation of balconies, porches and patios in the building design is encouraged for both practical and aesthetic value. These elements should be integrated to break up large wall masses, offset floor setbacks and add human scale to buildings.

Chimneys and Vents

Chimneys, as an architectural form, shall be simple and boldly projected. Natural stone veneer and articulation details are encouraged. It is recommended that exposed flues and extravagant metal fireplace caps not be used.

All vent stacks and pipes shall be colored to match the roof or wall material.

Gutters and Downspouts

Gutters and downspouts generally are a problem in snow country, due to freeze/thaw cycles, ice dams at eaves and snow and ice loads. Roof configurations and building and site planning should be designed to avoid the use of gutters and downspouts. When necessary, the gutter should be incorporated into the roof structure and not attach to or replace the fascia.

ATTACHED RESIDENTIAL *(continued)*

Mechanical Equipment

Roof mounted mechanical equipment shall be screened from view in a manner consistent with the building facade. Ground mounted mechanical equipment shall be screened from view with landscaping or fencing.

Skylights

Skylights should be designed as integral parts of the roof. Skylight framing material must be colored to match the roof. Flat skylights are encouraged.

Paving

Textures, patterns and colors are encouraged in the design of paved areas in public places. Modulation of surfaces should occur to define direction of walkways and location of major nodes such as recreation facilities, entries, etc. Large monolithic areas of single-color untextured paving are discouraged.

Solar Panels

Solar panels are to be integrated into the roof design, flush with the roof slope. Frames must be colored to match roof colors. Natural aluminum finish is strongly discouraged. Support solar equipment shall be enclosed and screened from view.

Awnings

Canvas or vinyl awnings of solid accent colors are permitted in moderation. Metal awnings are strongly discouraged.

Accessory Structures

Patio trellises and other exterior structures may be of stucco or wood as permitted by Town codes, with finishes complying with the overall color palette.

Parking and Service Lighting

Parking lots should be lit with a high enough intensity to discourage vandalism and help create security. The design of the poles and fixtures should be coordinated with other site lighting. Poles must be protected from auto bumpers, either by placing them far enough away from bumper overhangs or place the light standards on a 24" concrete pedestal. Hooded lights are recommended for parking areas to increase efficiency. Care should be taken in placing lights to avoid light penetration into upstairs windows of housing units or adjacent properties. (See "Lighting" in Chapter IV, Programs.)

ATTACHED RESIDENTIAL *(continued)*

Support Facilities

Any support buildings within the Attached Residential areas such as laundry facilities, recreation buildings and sales/lease offices shall be consistent in architectural design and form as previously illustrated for the residences. Temporary sales offices must be compatible with these standards in order to satisfy these design criteria.

Mailboxes

Where common mailbox services are provided, they should be located close to the project entry or near recreational facilities. The architectural character should be similar in form, materials and colors to the surrounding buildings. Mailbox locations must be approved by the U.S. Postal Service.

Trash Disposal

Trash bins should be fully enclosed within 6-foot brick, wood or native stone veneer walls and solid gates and should be softened with landscaping. Recommended locations include inside parking courts or at the end of parking bays. Location should be conveniently accessible for trash collection and maintenance.

VILLAGE CENTER DISTRICT

Purpose

The following section provides written and illustrated design direction related to the basic quality of office/retail building architecture color and scale. This portion of the manual addresses each of these elements in general terms and establishes the basic principles, which are expanded upon in much more detail through the application of the following "specific architectural guidelines." This section "paints the overall picture" for the design principles important for Gray's Crossing. They should not be viewed as standing alone but rather in concert with the more specific guidelines found in the subsequent sections of this manual.

Each guideline should be considered for how it applies to a given project. The illustrated examples are intended as images that communicate ideas and should not be viewed as specific design solutions.

VILLAGE CENTER DISTRICT *(continued)*

General Guidelines

The qualities and design elements for office/retail buildings that are most desirable include:

1. Richness of surface and texture;
2. Significant wall articulation, such as insets, canopies, porches, wing walls, bay windows, etc.;
3. Multi-planed, pitched roofs, often including dormers;
4. Roof overhangs;
5. Articulated mass and bulk;
6. Interesting and articulated walls;
7. Courtyards; and
8. Outside dining.



The following design elements are to be avoided:

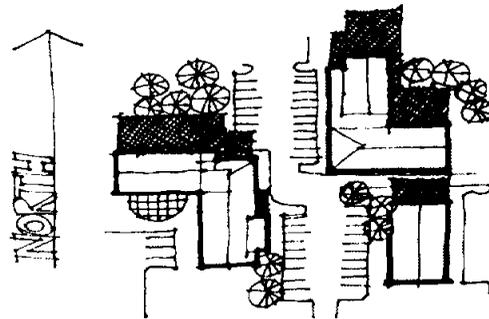
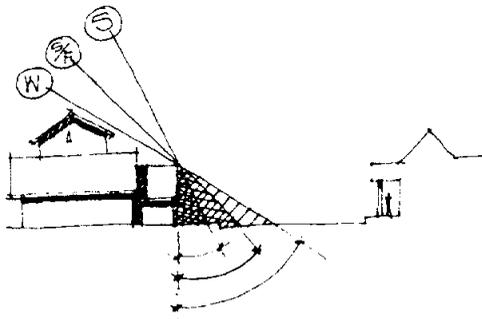
1. Highly reflective surfaces;
2. Large blank, unarticulated wall surfaces;
3. Unpainted concrete precision block walls;
4. Reflective glass;
5. Corrugated metal siding on the main facade;
6. Plastic siding;
7. Irregular, modernistic window shapes and/or rhythm;
8. Square “box-like” buildings;
9. Standing seam metal walls on the main façade; and
10. Mix of unrelated styles (i.e., rustic wood shingles and polished chrome).

VILLAGE CENTER DISTRICT *(continued)*

Height

Building heights should relate to open spaces to allow maximum sun and ventilation, protection from prevailing winds, enhance public views of surrounding mountains and forest and minimize obstruction of views from adjoining structures.

Height and scale of new development should be compatible with that of surrounding development. New development height should “transition” from the height of adjacent development to the maximum height of the proposed building.



North and south building and parking lot orientation allows solar exposure to parking lot reducing ice buildup and high maintenance costs.

Bulk

Large buildings that give the appearance of “square box” buildings are generally unattractive and detract from the overall scale characteristic in the region. There are several ways to reduce the appearance of excessive bulk in large buildings, such as:

1. Vary the planes of the exterior walls in depth and/or direction;
2. Vary the height of the building so that it appears to be divided into distinct massing elements;
3. Articulate the different parts of a building’s facade by use of color, arrangement of facade elements or a change in materials;
4. Use landscaping and architectural detailing at the ground level to lessen the impact of an otherwise bulky building; and
5. Avoid blank walls at the ground floor level. Utilize windows, wall articulation, and change in materials or other features.

VILLAGE CENTER DISTRICT *(continued)*

Scale

Scale, for purposes here, is the relationship between building size and the size of adjoining permanent structures. It is also how the proposed building's size relates to the size of a human being.

1. Building scale can be reduced through window patterns, structural bays, roof overhangs, porches siding, awnings, moldings, fixtures and details.
2. The scale of buildings should be related to adjacent pedestrian areas, streets and buildings.
3. Large dominating buildings should be broken up by using landscape materials, adding awnings, eaves, windows or other architectural ornamentation, and using combinations of complementary colors.
4. Utilize "infill" structures to create transitions in bulk and scale between large buildings and adjacent smaller buildings.



Color

1. The palette of colors from which to select are those that can be found in the natural environment of the surroundings.
2. The dominant color of new buildings should relate to the inherent color of the primary building's finish materials.
3. Large areas of intense white color should be avoided. While subdued colors usually work best as dominant overall color, a bright trim color might be appropriate if it can be shown to enhance the nearby visual environment.
4. The color palette chosen for a building should be compatible with the colors of adjacent buildings.
5. Wherever possible, minimize the number of colors appearing on the building exterior. Small office/retail buildings should use no more than three colors, except when requested by the applicant and approved in the Development Review process, on the basis that the design warrants additional colors.
6. Depending on the overall color scheme, accent colors may be effective in highlighting the dominant color by providing contrast or by harmonizing with the dominant color.

VILLAGE CENTER DISTRICT *(continued)*

7. Primary colors shall only be used to accent building elements, such as door and window frames and architectural details. Bright or intense colors (but not including fluorescent colors) can also be used to accent appropriate scale and proportion or to promote visual interest in harmony with the immediate environment.
8. In buildings of a particular historical character or architectural style, exterior color should be in keeping with the buildings proposed character and style.
9. Architectural detailing should be painted to complement the facade and tie in with adjacent buildings.
10. Accent colors for trim should be used sparingly and be limited in number for each building. Accent colors on adjacent buildings should be chosen to complement one another.

Architectural Design Guidelines

Exterior Walls

Buildings shall be designed to avoid a simple “box-like” structure. Horizontal or vertical wall articulation should be expressed through the use of full roofs, projecting wing walls, wall offsets, recessed entries, awnings, roof overhangs, second floor setbacks and/or covered arcades.



Freestanding buildings with walls at or less than 100 feet from a curb line should not have continuous, visually unbroken walls. The front plane of the wall shall be a maximum 40 feet in length, at which point horizontal or vertical articulation is required in order to be consistent with these guidelines. This articulation could be established through the use of varying front wall setbacks, multi-planed roofs, second floor setbacks, porches, arcades, awnings recessed entries, wing walls, roof overhangs, etc.

VILLAGE CENTER DISTRICT *(continued)*

Roofs

The roofline at the top of the structure shall not run in continuous plane for more than 60 feet without offsetting or joggling the roof plane in order to be consistent with the guidelines.

Nearly vertical roofs (A-frames and piecemeal mansard roofs used on a portion of the building perimeter only) will not meet the intent of the guidelines.

Roof materials that are high contrast, utilize brightly colored glazed tile or a highly reflective surface are discouraged.

Landscaping

Refer to the Landscape Design Guidelines herein.

Lighting

1. Maximum height for building and freestanding lighting shall be in compliance with the Town of Truckee Development Code.
2. If property is adjacent to a residential area or residentially zoned property, the lighting should be screened from these areas.
3. Lighting shall be directed away from adjacent roadways and shall not interfere
4. with traffic or create a safety hazard.
5. Upward lighting shall be minimized to the greatest extent possible.



See also "Lighting" in Programs, Chapter IV.

Signs

Refer to "Signs" in Programs, Chapter IV.

Programs

CHAPTER IV PROGRAMS

The purpose of this chapter is to set forth the standards applicable to certain common development programs within Gray's Crossing. The following programs are outlined in this chapter:

- Off-street parking;
- Signs;
- Lighting;
- Walls and fences;
- Grading and drainage; and
- Infrastructure.

OFF-STREET PARKING

PURPOSE

This Section is intended to establish comprehensive parking provisions to regulate the design of parking facilities and establish the number of parking spaces required for various land uses. Parking requirements are established to:

- Assure an adequate number of spaces are available to accommodate anticipated demand;
- Lessen traffic congestion and adverse impacts on surrounding properties;
- Encourage the use of alternative modes of transportation, such as trail systems and shuttles;
- Ensure access and maneuverability for emergency vehicles; and
- Provide loading and delivery facilities to support the project.

GENERAL PARKING GUIDELINES

Parking facilities shall be conveniently located and permanently maintained. Temporary reduction of parking spaces may be permitted to allow for snow storage during the winter months.

Development Standards and Parking Design

Off-street parking areas, driveways, and site access for the Residential, Village Center, and Recreation Districts will be designed and constructed in compliance with the Truckee Development Code (Section 18.48.70 and Section 18.50).

Required Number of Parking Spaces

1. Unless otherwise specified herein, references to acreage or square footage shall be considered gross acreage or gross square footage.
2. When the calculation of the required number of parking spaces requires a fractional parking space, any such fraction equal to or greater than one-half (1/2) shall be constructed as a whole and fractions less than one-half (1/2) shall be eliminated.
3. Shared parking reduction in Village Center: The Truckee Development Code (Section 18.48.050) allows a 25% reduction of the total parking spaces required where two (2) or more non-residential uses have distinct and differing peak traffic usage periods. A parking study analysis may be required.
4. Compact car spaces in Village Center: Parking lots may include compact car spaces for up to 25% of the total number of required spaces (per Truckee Development Code, Section 18.48.050). Size and markings shall be consistent with the Development Code.
5. Disabled/Handicapped Parking will be in compliance with Section 1129B of the California Building Code of Regulations, State of California, and Truckee Development Code.
6. The number of required spaces by Land Use is listed in the following Table.

PARKING REQUIREMENTS BY LAND USE	
Land Use District	Vehicle Spaces Required
Residential Single-family Homes..... Cottage Units..... Attached Units..... Secondary residential units.....	2 spaces. 2 spaces. studio and 1 bedroom units – 1.5 spaces per each unit with 1 space per unit in a fully enclosed garage. 2 bedrooms or more – 2 spaces per each unit, with 1 space per unit in a fully enclosed garage. Spaces not required to be in garage for affordable housing units. Guest parking – 25% of total required spaces. 2 spaces in addition to that required for the single-family dwelling.
Village Center Restaurants..... Retail Stores..... Employee Housing..... Lodging..... Offices/lofts.....	1 space per each 75 square feet of gross floor area for patrons, plus 1 space per each 300 square feet of service area, plus one space per each 100 square feet of outdoor dining area. Parking spaces for outdoor dining areas may be used for snow storage. 1 space per each 250 square feet of gross sales area, plus 1 space per each 600 square feet of storage area, and 1 space per each company vehicle. 1 space per each bed, plus 1 space per each 8 beds for guest parking; 1 space per each employee on largest shift. 1 space per each guest room, plus 1 space per each 2 employees on largest shift. 1 space per each 250 square feet of gross floor area for the first 5,000 square feet and 1 space per each 300 square feet thereafter.
Recreation Golf Course..... Golf Driving Range..... Clubhouse..... Health/Fitness Center.....	3 spaces per hole. 2 spaces per tee. 1 space per each 250 square feet of gross floor area. 1 space per each 250 square feet of gross floor area.

BICYCLE PARKING

Bicycle parking facilities will be provided in the Village Center, Attached Residential and employee housing buildings in compliance with the Truckee Development Code (Section 18.48.090).

VANPOOL/SHUTTLE BUS PROGRAM

A vanpool/shuttle bus program, approved by the Planning Commission, may qualify for a parking reduction of up to 20% on an approved Site Plan. The vanpool/shuttle bus program may be approved concurrently with the Site Plan. The vanpool/shuttle bus program must include all of the following elements (in any order) to qualify for the parking reduction:

1. Vanpool/shuttle bus parking facilities and identification of agency/entity providing the service;
2. The uses or businesses and parking locations for which the vanpool/shuttle bus program is applicable;
3. The anticipated schedule for vanpool/shuttle bus drop off and pick up;
4. Funding source(s); and
5. Implementation, enforcement and monitoring plan.

SIGNS

PURPOSE

This program provides the necessary guidance for the implementation of signage that meets the identification and informational needs of the community and is aesthetically compatible with the thematic and quality standards established for the Gray's Crossing Specific Plan area.

MASTER SIGN PROGRAM

Prior to the issuance of the first building permit within the Gray's Crossing Specific Plan area, a Master Sign Program, consistent with the Town's Development Code, shall be prepared for review and approval by the Town Planning Staff. Each land use designation will be addressed and include the items listed below:

1. Purpose statement and areas of applicability;
2. Permitted, conditionally permitted and prohibited signs;
3. Sign location by sign type;
4. Acceptable installation methods;
5. Sign specifications, including maximum dimensions and acceptable copy style(s),
6. materials, and colors;
7. Number of signs permitted;
8. Maintenance requirements and responsible agencies; and
9. Sign definitions.

GENERAL GUIDELINES

The General Guidelines listed below provide the basic criteria for the design and implementation of signs throughout the Specific Plan area.

Sign Design

1. All signage should be constructed with high quality materials and finishes and fabrication.
2. All signs and their supporting structures should be enclosed and maintained in good condition. Exposed hardware should be finished in a manner consistent with quality fabrication practices.
3. In order to prevent staining of architectural surfaces, non-corrosive materials should be used on all exterior signs.

4. All signage within private uses should maintain a minimum 5-foot setback from any public right-of-way.
5. The number and size of signs should be kept to a minimum. Only signs necessary to clearly communicate the message intended shall be permitted.

Sign Maintenance

1. All signs shall be maintained in a safe and attractive condition at all times. Upon notice from the Town of Truckee, a tenant will be required to refurbish, within thirty (30) days, any signage which does not meet the standards as stated within this Master Sign Program. Damaged signs, from either a natural or man caused occurrence shall be replaced within thirty (30) days.
2. Any sign illumination malfunction shall be replaced or remedied within (ten) 10 days.

Sign Illumination

1. All signs and their illumination systems should utilize the minimum amount of energy through the use of energy saving design techniques, equipment and materials.
2. All exterior sign illumination shall be consistent with the Lighting section of this chapter, except as otherwise stated within this Master Sign Program.

Prohibited Signs

1. Any signage not specifically permitted by this Master Sign Program or the Town of Truckee, is prohibited.
2. No sign, part or attachments thereof, shall flash, scintillate, move, change color, appear to change color or intensity or contain any part or attachment that does the same, unless approved by the Community Development Director.
3. Pole signs to identify individual users are prohibited.

Entryway Signs-Village Center and Recreation Districts

1. Entryway signage should be provided at each roadway entrance to the Village Center and Recreation Districts within the Gray's Crossing Specific Plan.
2. Entryway signage may consist of monumentation, garden or screen walls or sculptural elements that depict the thematic architecture of the Specific Plan area.
3. Entryway signage should be integrated into the landscape design of the immediate area.
4. Kiosk or multi-panel signage is allowed for the identification of multiple businesses as viewed from State Highway 89 North and Interstate 80 to reduce the number of individual signs.

Street Identification

1. Street identification signs shall be provided at each roadway intersection and be clearly visible and in conformance with subdivision and street standards of the Town of Truckee.
2. Street identification signs should be designed to enhance the overall streetscape appearance. Compatibility with the architectural themes established for the Specific Plan area is encouraged.

Vehicular Roadway/Bicycle Pathway Directional Signs

Directional signs (e.g., stop signs and yield signs) for vehicular roadways shall be implemented in accordance with the state vehicle code. Opportunities for architectural enhancement of directional sign standards should be explored during project design.

Community Kiosks

1. Kiosks may be located within the public right-of-way to provide communication tools for the area.
2. A Master Kiosk Program should be prepared, prior to development, to identify locations, design and maintenance conditions. The program should also provide provisions for public use and maintenance.
3. Kiosks should be designed as an integral part of the architectural and landscape theme.

RESIDENTIAL NEIGHBORHOOD SIGNS

Neighborhoods

1. Entryway signage should be clearly visible at each key entry point to a residential area and architecturally compatible with the residential neighborhood it serves and incorporated within a landscape area.
2. A monument sign is encouraged for entryway signage.
3. Illumination for entryway signage should be distinct and in accordance with the Lighting section of this chapter.

Residence Identification

1. An address number shall be provided for each residential dwelling unit that is designed as an integral part of the immediate architecture and landscaping.
2. Address numbers should be clearly visible from the public right-of-way and scaled appropriately for the residential structure it serves.

NON-RESIDENTIAL USES

Design

1. All signage should promote and enhance the architectural and landscape design of the individual project it serves.
2. Sign copy should be kept to a minimum and should be appropriately scaled to the architectural design of the structure and tenant space it serves.
3. Unless wall mounted, all signs should be architecturally treated equally on both sides.
4. No advertising shall appear on signs except as established within the Master Sign Program for the individual project it serves.

Tenant Signs

1. Individual tenant signs should be designed as an integral part of the architecture.
2. Individual tenant signs should be limited to the tenant name and logo.
3. The internal illumination of individual sign letters is encouraged.
4. All tenant signs should be removed when business goods are no longer sold or produced or services are no longer provided.
5. Tenant signs need not be directly attached to the lease space to which they refer.

Directory Signs

1. Directories should be provided in multi tenant centers to aid in the identification of tenants and specific facilities.
2. Directories should be strategically placed to minimize their use and maximize their effectiveness.

TEMPORARY SIGNS

General Guidelines

1. Temporary signs should be architecturally compatible with the standards within the architectural guidelines of this Master Sign Plan.
2. Temporary signs may not be illuminated.
3. Temporary signs are not to be decorated with flags, banners, balloons or similar devices.

LIGHTING PLAN

PURPOSE

This summarizes the lighting design objectives, lighting design criteria and recommended lighting solutions as detailed in the Lighting Master Plan (Appendix A) and developed by East West Partners, Hart|Howerton and Francis Krahe & Associates, Inc.

OBJECTIVES

- Maintain visibility of stars in the night sky.
- Establish a warm, inviting character.
- Create a unique identity, responding to local vernacular.
- Provide functional lighting for safety.
- Minimize light during non-active hours (11pm–dawn).



DESIGN STANDARDS

Design standards were based on energy efficiency, durability, and safety. The criteria used were developed from following sources and codes:

- Certification program for Leadership in Energy and Environmental Design (LEED),
- International Dark Sky Association (IDA),
- Illuminating Engineering Society of North America (IESNA),
- National Electric Code, (NEC),
- National Fire Protection Association (NFPA),
- American Disabilities Act (ADA),
- State Codes, and
- Truckee Municipal Code.

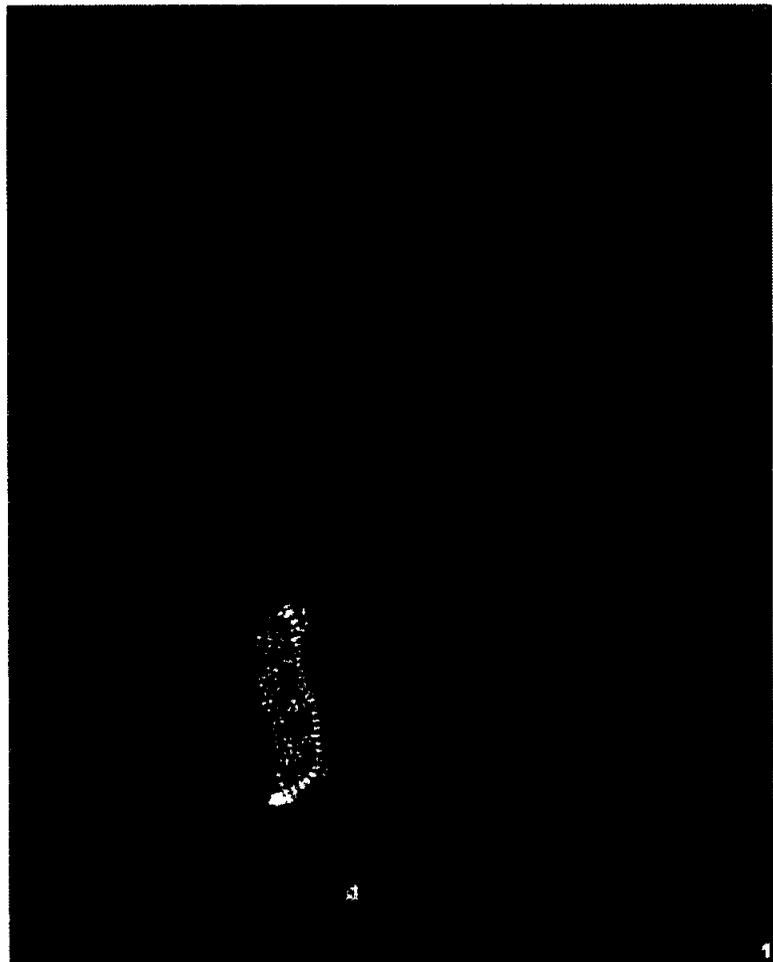
Leadership in Energy and Environmental Design (LEED)

Gray's Crossing will be designed to meet the LEED recommendations for Light Pollution Reduction. The objectives are to maintain night sky visibility, reduce development impact on nocturnal environments, and eliminate light trespass between building sites. To achieve these goals, Gray's Crossing will comply with the following requirements:

1. Meet light levels and uniformity ratios recommended by the Illuminating Engineering Society of North America (IESNA).
2. Exterior light brighter than 1000 lumens will be "shielded".
3. Exterior lights greater than 3500 lumens will be "Full Cutoff" type.

SCOPE OF LIGHTING PLAN

- Site entry;
- Main street and secondary streets;
- Parking;
- Retail;
- Hospitality;
- Gas Station; and
- Residential.



LIGHTING DESIGN INTENT



Design Style

Lighting will support the architectural style and highlight quality material and components.

Color Quality

Warm lighting will delineate comfortable and inviting environments.



Hierarchy

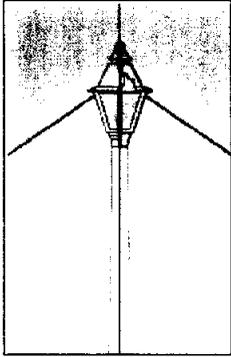
Lighting intensity is organized to identify focal points.

Transparency

The lighting intensities will extend visibility and create transparency, from area of lower to higher brightness.



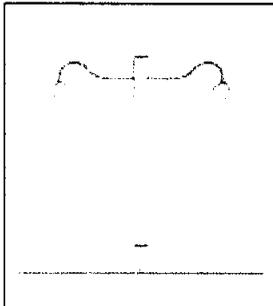
POLE LIGHTING



- Shielded light fixture of full cutoff style.
- Shielded fixture throws light down and minimized light pollution and glare.
- Minimizes light pollution and glare.



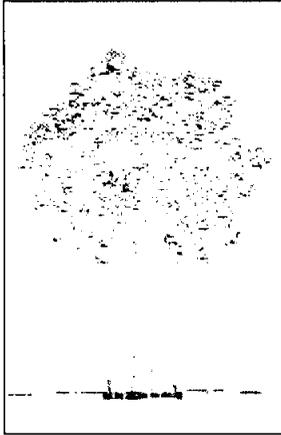
SIGN LIGHTING



- Shielded light fixture conceals lamp.
- Fixture above aimed downward.
- Minimized light pollution and glare.
- Front lighted signage.



TREE UPLIGHTING



- Halogen uplights equal to or less than 50 watts or 1000 lumens.
- Uplights directly below and captured by canopy of tree.
- Minimizes light pollution and glare.



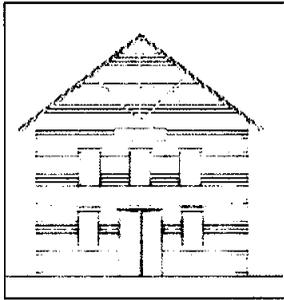
TREE DOWNLIGHTING



- Halogen downlights equal to or less than 75 watts or 1300 lumens.
- Light fixture aimed straight down at the ground.
- Minimizes light pollution and glare.



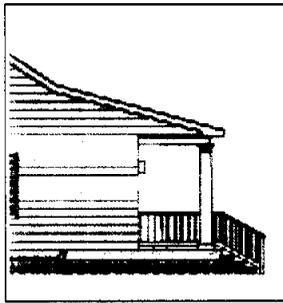
FACADE LIGHTING



- Shield fixture conceals lamp.
- Fixture throws light up under and within architecture and on to ground below.
- Minimizes light pollution and glare.



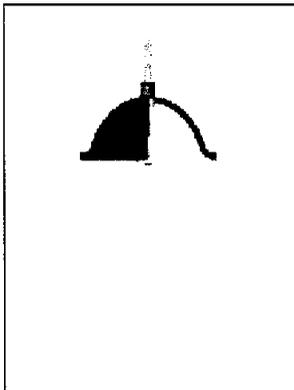
BUILDING MOUNTED FIXTURES



- Shielded fixture conceals lamp.
- Fixture throws light up within architecture and on to ground below.
- Minimized light pollution and glare.



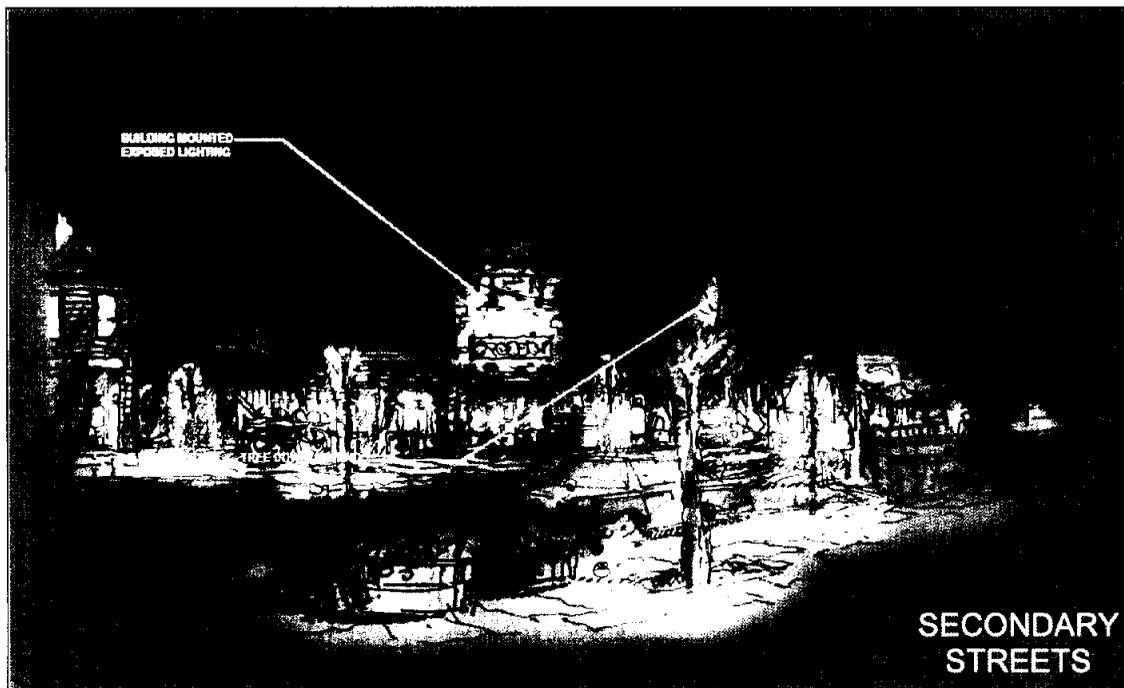
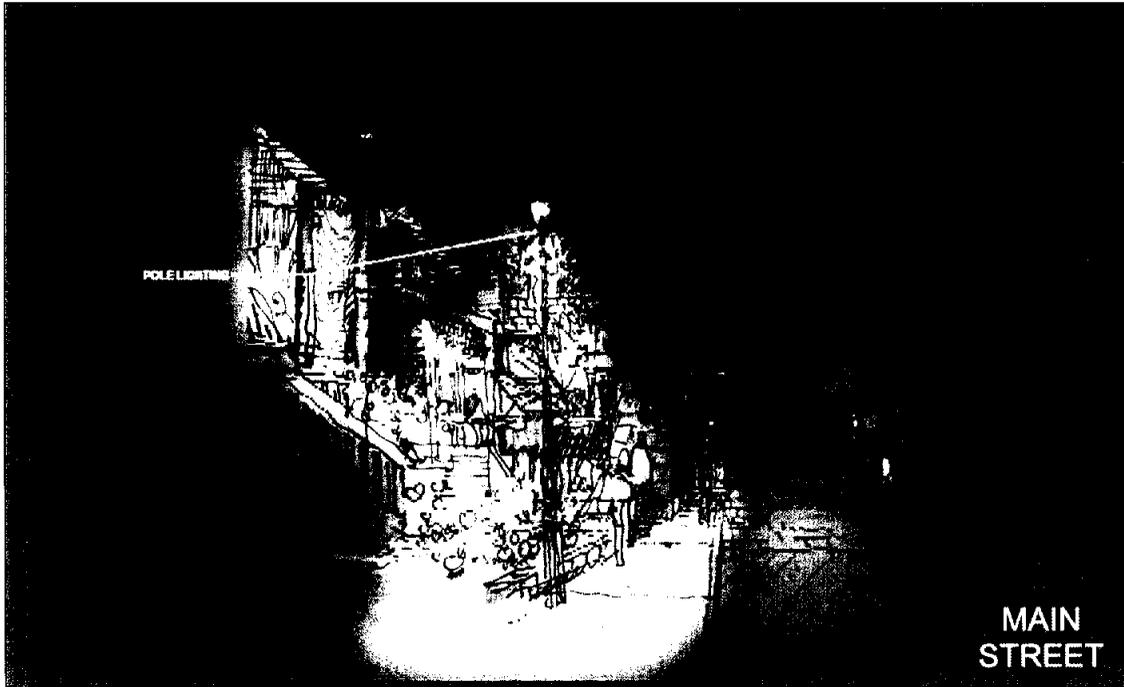
FESTIVE LIGHTING TECHNIQUES



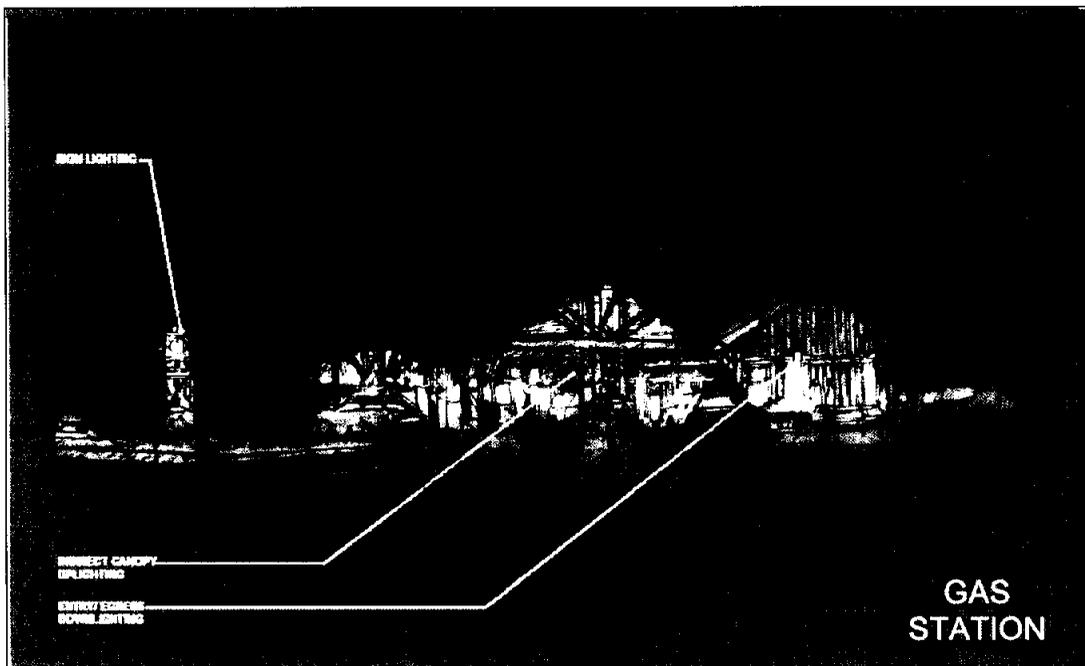
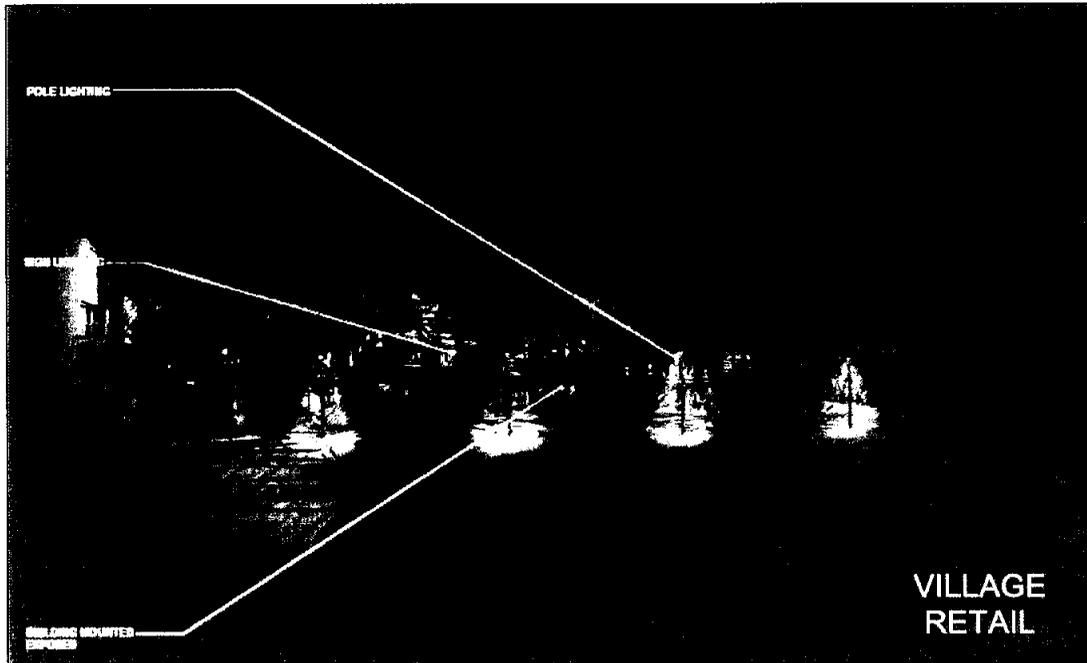
- Shielded fixture conceals source, and throws light downward only.
- Lamps are equal to or less than 5 watts.
- Light output less than 35 watts or 1000 lumens per tree or building component.
- Minimizes light pollution and glare

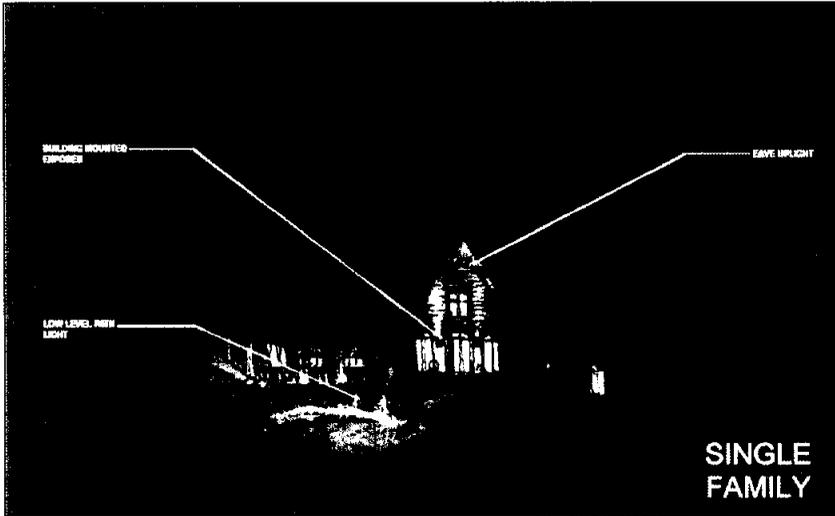


LIGHTING EXHIBITS
STREETS

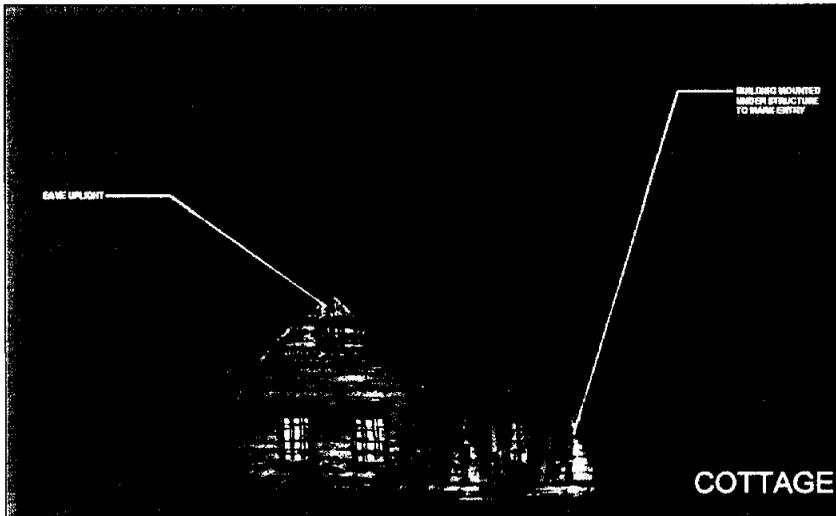
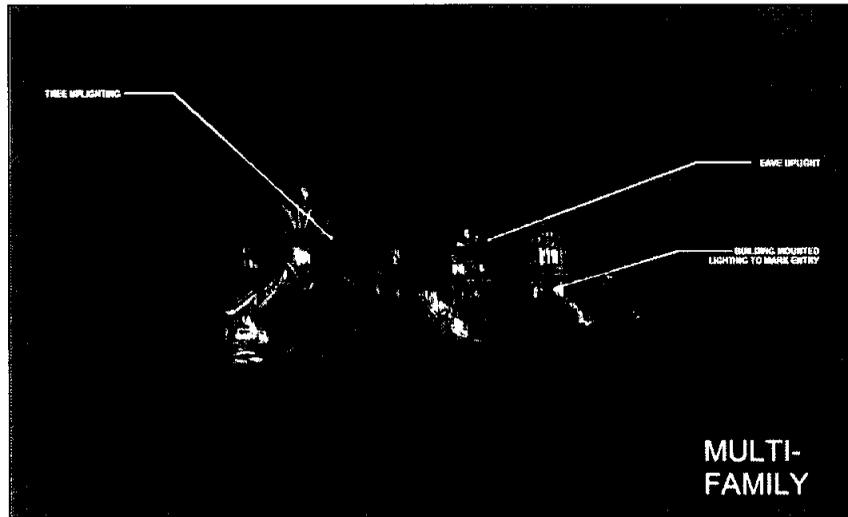


VILLAGE CENTER DISTRICT





RESIDENTIAL
DISTRICTS



GRADING AND DRAINAGE

GRADING

The Grading Program is intended to ensure that excavation and fill operations are completed in a coordinated and efficient manner that will enhance project aesthetics.

OPERATIONAL GUIDELINES

1. Due to the topographic nature of the site, extensive grading operations will not be necessary to accommodate the project. Primary grading will occur in the development of the Village Center areas. Excess cut material should be used in the creation of berming and other topographical variations within the development area to add visual interest throughout project landscaping.
2. Grading operations within the Specific Plan area shall follow the recommendations provided in the geotechnical investigations required prior to any development. Geotechnical recommendations shall include measures to mitigate potential impacts and address, as appropriate, the following:
 - a. Expansive soils;
 - b. High groundwater levels;
 - c. Existing unsuitable soils and fill;
 - d. Site preparation;
 - e. Temporary excavations; and
 - f. Temporary soil erosion techniques.
3. To the extent feasible any grading within the Specific Plan area shall balance cut and fill on-site. If the transports of cut and/or fill is necessary, transport operations should be consistent with Town ordinances and be identified in the Site/Tentative Map review.
4. Grading activities shall be phased with development to the maximum extent possible and be consistent with the overall conceptual plan for the entire Specific Plan area.

EROSION CONTROL

Guidelines

An Erosion and Sedimentation Control Plan shall be prepared prior to the issuance of a grading permit to ensure adequate erosion control protection during grading operations. Details of that Plan shall be in conformance with the Best Management Practices Handbook Standard, Volume II, Tahoe Regional Planning Agency, November 1988 or as amended.

STORM DRAINAGE

Purpose

The Storm Drainage Program is intended to provide comprehensive drainage facilities to adequately serve the Specific Plan area consistent with the provisions of the State of California, Regional Water Control Board, Lahontan Region, Discharge Standards or as amended.

General Guidelines

Prior to any development, a Master Drainage Plan shall be submitted with each Development

Plan that incorporates the following:

1. Use of greenbelts and landscaped areas to convey and store storm water runoff;
2. Use of retention basins to contain runoff prior to its discharge into any drainage course; and
3. Integrate "biological treatment processes" within the greenbelt and landscape area per the standards of the California Regional Water Control Board, Lahontan Region and the recommendations of Volume II of the Best Management Practices Handbook of the Tahoe Regional Planning Agency, November 1988 or as amended.

INFRASTRUCTURE

PURPOSE

The Infrastructure Program is intended to ensure that adequate infrastructure is provided to the Gray's Crossing Specific Plan area. The Preliminary Utility Plan (at the end of this Chapter), conceptually depicts proposed water, sewer, electrical and roadway alignments. Final approval of these improvements, including location and size, rest with the utility districts providing services, i.e., the Truckee Donner Public Utility District and the Truckee Sanitary District. Utilities and roadways will be constructed in phases as development within the Specific Plan area is constructed. The size, type and amount of infrastructure shall coincide with the amount of development proposed in any one phase, provided each phase of improvement meets each District's regulations.

WATER DISTRIBUTION SYSTEM

General Guidelines

1. A water distribution system for the Specific Plan area shall be designed in conformance with the provisions of the Truckee Donner Public Utility District Master Plan.
2. Water facilities needed to accommodate the demand within the Specific Plan area shall be installed as each phase develops. Storage tanks with booster stations and distribution lines of 6, 8, 10 and 12 inch water mains will be required. The final configuration of storage may vary depending upon water quantity requirements of development phasing.

The water distribution system for the Gray's Crossing Specific Plan area will be designed to be compatible with the future regional distribution facilities, as depicted within the Town's General Plan for the project area. Each phase of development within the Plan area shall size construct that portion of the system as determined by the District. Reimbursement agreements may be considered subject to District Standards.

3. To supply water service to the Gray's Crossing Specific Plan area will require approximately 400,000 gallons of storage capacity depending on fire flow requirements based on build size material. The water source will be provided by a combination of on- or off-site wells drilled and maintained by the Truckee Donner Public Utility District. Service to the individual development components within the Specific Plan area will be via water distribution lines in various sizes located within the roadway right-of-ways.

The Truckee Donner Public Utilities District is constructing a new storage tank on their property located between Interstate 80 and the Pioneer Valley Convention Center. This facility will serve the area and Gray's Crossing will pay their fair share of the cost.

The Preliminary Utility Plan (at the end of this Chapter), depicts the general location for utility lines to service to this project.

SEWAGE COLLECTION SYSTEM

Purpose

The Sanitary Sewage Collection System shall be designed in conformance with the provisions of the Truckee Sanitary District. The system shall be designed to accommodate the uses proposed within the Gray's Crossing Specific Plan area, as well as appropriate sizing for adjacent properties. Prior to development, annexation to the Truckee Sanitary District will be required to accommodate the lands within the Gray's Crossing Specific Plan area. The Sanitary Sewage Collection System shall incorporate the following:

1. Quantification of anticipated sewage generation per each land use zone within the Specific Plan area;
2. Appropriate line sizing to accommodate flows on-site and from adjacent properties to the north and west;
3. Coordination with the Truckee Sanitary District to determine pipe sizing and location and appropriate crossing for the main sewer line under Interstate 80;
4. Phasing of the Sewage Collection System shall generally be in accordance with the phased development areas within the Specific Plan;
5. Prior to development of the first phase, a comprehensive collection system design shall be reviewed and approved by the Truckee Sanitary District and annexation shall be completed for the entire property within the Specific Plan area;
6. The two preferred off-site sewer connection locations to transfer waste water from the site are the existing bridge crossing in the Martis Valley area, just south of the Olympic Heights Subdivision and the SAD 6 crossing under the Truckee River that outflows from the Glenshire Subdivision. To connect to these locations will require off-site improvements starting from the southeast property corner of the plan area paralleling the west side of Section 12 down to Glenshire Drive. From this location, the sewer line would be extended either easterly toward the SAD 6 outfall crossing or westerly toward the newly installed bridge crossing the Truckee River.

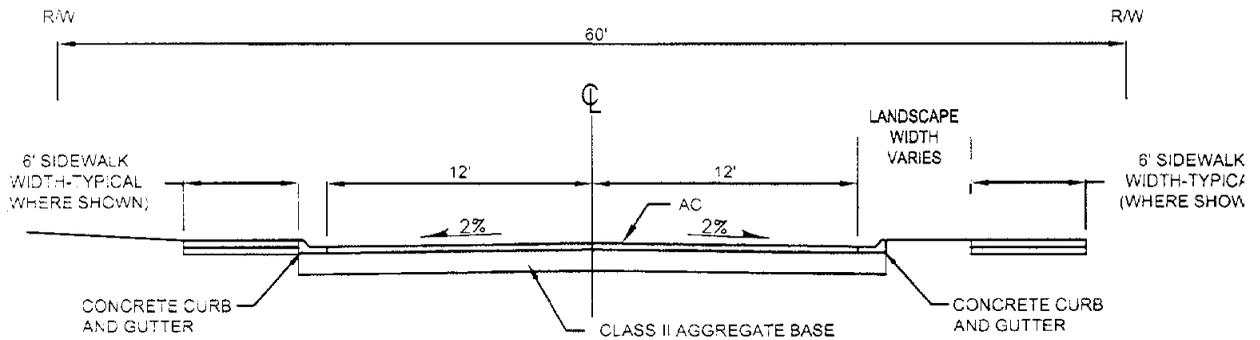
The Preliminary Utility Plan (at this end of this Chapter), depicts the location of on-site lines. Like the requirements of the Truckee Donner Public Utility District, these facilities must be approved in size and location by the District prior to construction.

In addition to the above, a limited alternative source for sewer lines, depending on the volume of affluent generated, could be the existing crossing on Interstate 80, west of State Highway 89. However, this line has limitations due to the size of pipe and its intersection with Tahoe Truckee Sanitation Agency main line south of the Truckee River.

ROAD CIRCULATION

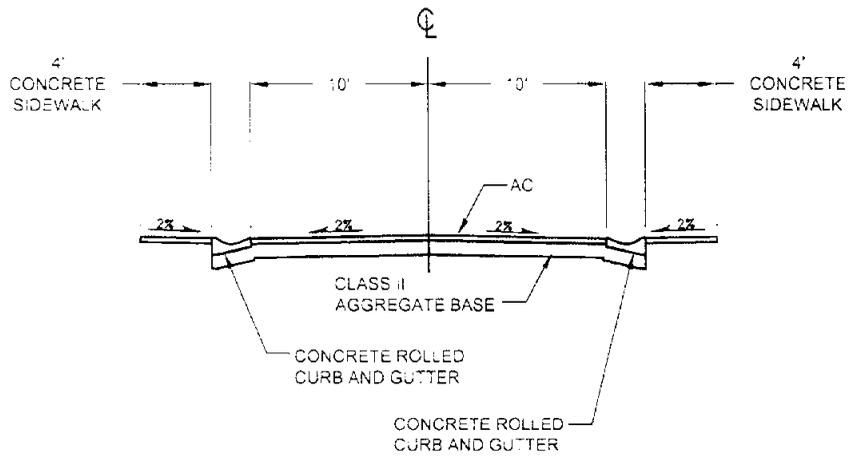
A network of road systems has been designed through the Specific Plan area. The main accesses to the Specific Plan area are State Highway 89 and Prosser Dam Road. It is anticipated a full left-turn and/or a controlled intersection, will be necessary to accommodate traffic generated from current and projected traffic at this intersection. In addition to the Road Standards Sections, depicted on the following pages, the roadway network is shown on the Tentative and Development Maps within this Specific Plan. When Development Permits are requested for each phase the Site Plan and/or Tentative Map shall demonstrate the following:

1. All roads shall operate at a level of service (LOS) consistent with the Town of Truckee Public Facilities Master Plan or performance standards for circulation facilities.
2. Continuous walkways shall be provided within the Residential and Attached Residential Districts. The walkways shall be conveniently linked with other land uses. Pedestrian paths should provide direct access from Residential Districts to the Village Center, open space areas, public schools and parks.
3. Road Standards for the various land uses depicted in the Specific Plan area are shown on the following pages.



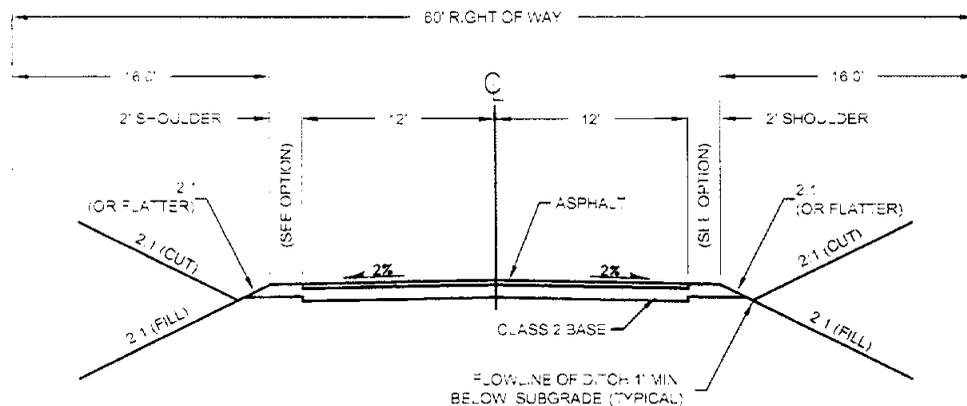
TYPICAL VILLAGE ROAD SECTION

N.T.S.



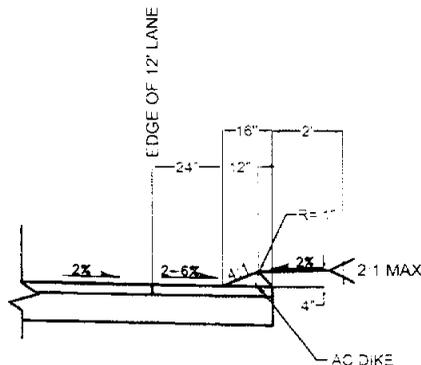
TYPICAL ROADWAY SECTION

"COTTAGE AREA"



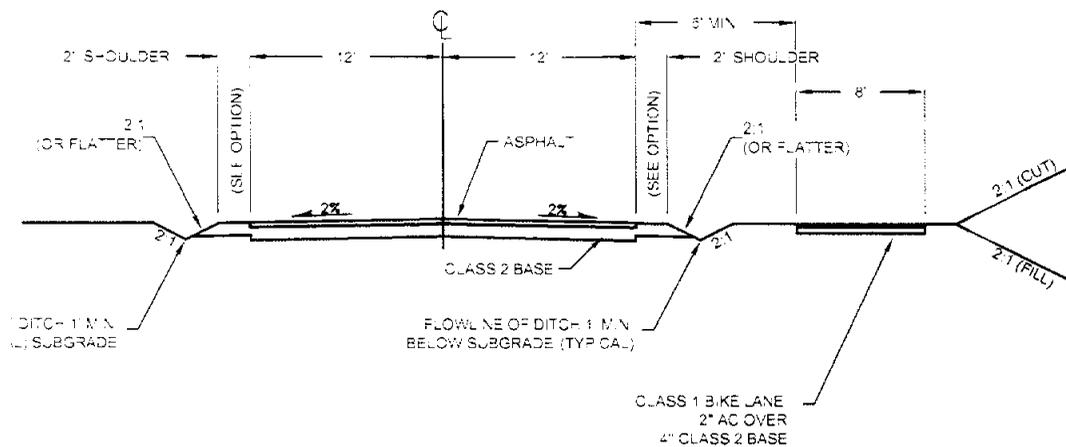
TYPICAL ROADWAY SECTION

N.T.S.



TYPE B AC DIKE OPTION

(CUT SLOPE ONLY)
N.T.S.



TYPICAL ROADWAY & BIKE LANE SECTION

ELECTRICAL DISTRIBUTION SYSTEM

General Guidelines

1. Electrical facilities shall be provided to adequately serve the project as it develops and shall be designed to accommodate ultimate development.
2. Electrical facilities shall be designed and constructed in accordance with Truckee Donner Public Utility District Standards.
3. Electrical distribution shall be located underground within designated utility easements. Overhead lines shall not be permitted to access electrical substation facilities off-site.
4. Project developers shall coordinate with the Truckee Donner Public Utility District for phasing and requirements of electrical services.
5. Electrical service to the site can be provided from two locations:
 - a. Truckee Sub-Station – 7,500 KWA Sub-Station; and
 - b. Martis Sub-Station, by Truckee Sanitary District's holding pond – 15,000 KWA sub-station.

An interim 3-phase extension from the power poles west of State Highway 89, over the highway and along the power line easement fronting the south property line is available. This would allow service to the Village Center and some residential development. As residential development builds out, a loop system connecting this power source to the Martis Sub-Station will be needed. This will require overhead facilities running out of the southeast corner of the property southwest and along the side of Section 12 down to Glenshire Drive and eventually to the Martis Sub-Station.

Electrical services provided on-site will be underground and installed in the roadways (see Preliminary Utility Plan (at this end of this Chapter), for location of these facilities).

6. Relocate existing high voltage transmission lines to the easterly portion of the site.

SOLID WASTE DISPOSAL PLAN

A Solid Waste Disposal Plan shall be prepared for the Specific Plan area prior to development of the first phase. The plan shall incorporate the requirements of A.B. 939 and include the following:

1. The Solid Waste Disposal Plan shall address the implementation of a redemption collection facility for recycled materials, including location, hours of operation, operational procedures and facility specifications. The facility may be mobile, with regularly scheduled days and hours of operation.

2. The Solid Waste Disposal Plan shall also address the use of a mulching facility, including location, timing of implementation, operational procedures and facility specifications. Mulching facilities should be screened from public view.
3. A curbside collection program should be implemented, if feasible and include the following:
 - a. Attractive curbside collection containers for homes in the Residential District;
 - b. Adequate storage area for recycling adjacent to refuse storage dumpsters within Attached Residential and Village Center Districts; and
 - c. Procedures for the clean up of fugitive materials from collection operations.
4. A solid waste reduction and recycling plan shall be prepared for individual businesses or business complexes. The Solid Waste Reduction Plan and recycling procedures shall be required prior to the issuance of an occupancy permit for the business or complex. The plan shall provide an analysis of anticipated waste stream, source reduction schemes and recycling procedures that will be implemented. Provisions for monitoring the effectiveness of the plan will also be required.

Truckee's General Plan and Policy Compliance

CHAPTER V COMPLIANCE WITH TRUCKEE'S GENERAL PLAN AND POLICIES

SUMMARY OF FUTURE NEEDS

The land use element of the Truckee General Plan sets forth "specific goals and policies to guide the intensity, location and distribution of land uses for the next twenty years."¹ Using the technical expertise of land use planners, biologists, acoustical engineers, traffic engineers and economists, the existing physical and economic conditions of Truckee were quantified and documented in a variety of technical background reports. Using the data within the technical background and past development trends for the area, projections of future population increases, environmental conditions and economic conditions were determined. To direct future development trends a community Vision Statement was adopted to provide overall guidance for the development of a General Plan. The combination of the Vision Statement and technical information was used to develop and adopt a projected growth rate anticipated for the General Plan planning period.

The proposed land use designations in the Gray's Crossing Specific Plan were selected to be consistent with and to accommodate the growth authorized by the Truckee General Plan. Those projections are shown on Table 1 below.

TABLE 1 GENERAL PLAN PROJECTED GROWTH BUILD-OUT				
Description	Existing Development 1995	Additional New Growth Projected for Year 2015	Total Projected Development for Year 2015* <i>(Existing Development plus Additional New Growth Projected)</i>	General Plan Buildout
Housing Units	9,039	4,682	13,271	17,850
Occupied Housing Units	4,283	1,981	6,264	8,508
Population	11,291	5,962	17,253	22,980
Commercial/Industrial (1,000 sq. ft.)	2,300	1,579	3,879	5,675
Employee	5,225	2,884	8,109	12,460
* Economic Projects are for the year 2015 and were based on information in the Town of Truckee Economic Analysis, SR Hoffman Associated, October 1995				

¹ Town of Truckee General Plan, dated February 15, 1996, Chapter II, Page 37, Section Introduction, Land Use Element

To accommodate this growth, in conformance with the Vision Statement, the General Plan has adopted a variety of land use designations that disperse the various land use categories over the 19,520 acres that incorporate the boundaries of the Town of Truckee.

The following table indicates the various land use designations, acreage allocated to each and the percentile breakdown:

TABLE 2 TOWN OF TRUCKEE LAND USE DESIGNATIONS		
Type	Acres	Percent
Open Space	3,165	16.0%
Public	3,257	17.0%
Industrial	154	1.0%
Commercial	295	1.5%
Residential	7,226	37.0%
Planned Community	4,858	25.0%
Downtown Study Area	481	2.0%
Special Study Area	84	0.5%
Total	19,520	100.0%

In summary, the General Plan states the following:

1. Population: Truckee's population is projected to increase by 5,962 people by the year 2015.
2. 4,682 Additional Housing Units are Projected: The number of housing units in the Truckee area is expected to increase from its present 9,039 units to over 13,271 units by the year 2015.
3. 2,884 New Jobs: The number of persons employed in the Truckee area will increase from 5,225 to 8,109 by the year 2015.

**COMPARISON OF GENERAL PLAN POLICIES
AND SPECIFIC PLAN STANDARDS**

The Land Use element of Truckee's General Plan sets forth building and intensity specifications for each land use designation and Planned Communities for private lands. These intensity factors range from units per acre to Floor Area Ratios (FAR).

Each Planned Community has specific land use policies set forth in the General Plan. Gray's Crossing land use policies are set forth under PC-2 Policies 1 through 15 and specify the goals and parameters to be included in developing a Specific Plan for this site. These specific policies stipulate how the individual land use designations must be applied to the particular sites and equates to a land use intensity that in some cases is equal to and in many cases, is less than the land use density/intensity specified in the base designations. These density and intensity factors for each of the General Plan land use designations and each Planned Community are outlined in the Appendix A, Table A.1 of the General Plan and specifies the applicable acreage and anticipated build-out capacity.

The build-out capacity for PC-2 (Gray's Crossing project) anticipates up to 818 acres of land being involved, allowing 600 housing units with occupied units of 288 with a base population of 780 and a 400,000 square foot maximum non-residential square foot limitation and up to 800 employees on the site.

Table 2.4 of the Land Use Element of the General Plan sets a maximum limit of retail and office square footage at 175,000 square feet. This leaves a maximum of 225,000 square feet for lodging and recreational buildings.

The following is a list of applicable General Plan Goals and Policies that pertain to the Gray's Crossing Specific Plan. The following is an assessment of how the Plan complies and implements those goals and policies:

Truckee General Plan

Gray's Crossing Specific Plan

Land Use

Land Use Goal 1

Provide an adequate amount of land designated for residential, commercial and industrial uses to meet demand within the life of the Plan.

As described in the Introduction to the General Plan and in the Introduction to the Land Use Element, the Land Use Diagram was developed to meet this goal by accommodating growth projections for the year 2015. The following policies will ensure that the above goal is implemented as new development occurs.

Land Use Goal 2

Create efficient land use patterns that reduce environmental impacts and minimize the potential for residential and commercial sprawl.

Table 2.5 on Page 48 of Truckee's General Plan estimates the projected growth anticipated by the year 2015. Land use designations have been provided to accommodate this growth. The land use designations are dispersed around existing development and in the three planned communities. The Gray's Crossing project is within Planned Community Two (PC-2). Considering the amount of land allocated to accommodate future growth, the Gray's Crossing project provides 13% of the anticipated open space and 13% of the estimated residential units for the projected population.

The Gray's Crossing Specific Plan project area is bordered on the north, west and south by residential development. These developments are served by existing roadways, water and power distribution systems. Permitting development contiguous to existing development that utilizes existing roadway systems does not constitute sprawl. Allowing development to occur adjacent to existing development where new development has the potential to bring additional services and supply community needs, creates an effective solution oriented land use pattern.

Land Use Policy 2.3

To provide for projected population growth in an efficient manner, accommodate development at the highest densities in infill areas, consistent with goals for environmental protection and land use compatibility.

Infill areas are vacant or underutilized lands (usually individual lots) in areas that are largely developed within the Residential and Commercial categories.

The Gray's Crossing project can be considered infill property. Being surrounded on three sides by subdivisions, the preponderance of the project would occur adjacent to these existing developments. The Gray's Crossing project utilizes an existing roadway system and provides the opportunity for pedestrian and bicycle linkages to adjacent properties. Gray's Crossing provides a variety of services to properties north of Interstate 80 in a convenient consolidated and centralized location. In providing these amenities, the project has also designated 55% of its land to open space.

Land Use Policy 2.5

Analyze job creation opportunities as the economy shifts from an industrial base to a tourist base. Pursue such opportunities through appropriate economic development programs.

The Gray's Crossing Specific Plan, with its requirements to provide a destination recreation community and mix of land uses, has the potential to create over 168 permanent jobs. In addition to each permanent job, two to three spin-off jobs could be created as a result of this development. The creation of this many permanent jobs has the potential to substantially increase the economic viability of Truckee. Gray's Crossing provides the land use designations and mixed uses to successfully transition an industrial based economy toward a tourist-based economy.

Land Use Goal 3

Coordinate land development with provision of services and infrastructure.

Build-out of the Gray's Crossing Specific Plan will be constructed in phases. Each phase will extend water, sewer and power facilities in coordination with the District Standards of Truckee Donner Public Utility District and Truckee Sanitary District.

Land Use Policy 3.1

Work with all special districts, including the Tahoe-Truckee Unified School District, to ensure that development within the Town is coordinated with provision of services.

The Gray's Crossing project has already dedicated 35.7 acres of land to provide a middle school site for the Tahoe-Truckee Unified School District. In addition, 9.5 acres of land is available for the Truckee Donner Recreation and Park District.

Land Use Policy 3.2

Work with the applicable special districts to develop a program allowing transfer of rights to service from areas designated Resource Conservation/Open Space or Open Space Recreation to areas designated Residential, High Density Residential, Commercial, Industrial or Planned Community.

The previous property owner, Hopkins, LLC also owns property east of the Gray's Crossing site. These lands were designated in the Truckee General Plan as Resource Conservation/Open Space. These properties were within the boundaries of the Truckee Sanitary District. Prior to Truckee's incorporation, the Truckee Sanitary District formed an Assessment District (Assessment District No. 6). These properties were allocated 61 sewer assessments. The total cost of these sewer hook-ups was approximately \$70,206. Since the incorporation and adoption of the General Plan, these lands have been down zoned from their previous 2-acre minimum parcel sizes to 80-acre minimum parcel sizes. This makes the assessments on the property unusable. Hopkins, LLC annexed the PC-2 Gray's Crossing land to the Truckee Sanitary District and redistributed the assessments to the land within the Gray's Crossing boundaries.

Some landowners have been paying for rights to service, for example, sewer assessments, that would allow service for more units than would be allowed under this General Plan. If the service districts allow these rights to service to be transferred to areas where the Town will allow the units to be developed, landowners could be compensated for their assessment without densities in these areas being increased.

Land Use Policy 3.3

Approve rezoning and development permits only when adequate services, as defined by Table 2.6, are available or when a program to provide services has been approved by the applicable District and the Town of Truckee. The matrix in Table 2.6 (Service Standards and Timing) identifies standards of services for new development.

After adoption of the Specific Plan and prior to any physical development of the property, development agreements and service extensions in compliance with the appropriate District's regulations for sewer, water and utility purposes will be established.

Land Use Policy 3.4

Require that sewer is provided for all new residential subdivisions creating more than four lots and all-new commercial uses. Existing legal lots in areas currently without sewer may be developed with residential uses using septic systems with the approval of the appropriate health and environmental agencies.

All properties within the Gray's Crossing development will be served with sewer facilities provided by the Truckee Sanitary District.

Land Use Goal 4

Encourage a mix of land uses in the Town to promote a vibrant community and to reduce traffic, while addressing the need to minimize land use conflicts.

The Gray's Crossing project provides a mix of uses consistent with designations specified in the General Plan. Currently, no retail services are provided north of Interstate 80. Providing office space and neighborhood retail north of the highway will help intercept traffic that is now traveling to the west side of the Town for retail services. This distribution of land use can help reduce congestion at the west end of Town.

Land Use Policy 4.2

Allow live-work uses in Commercial and Industrial areas and accommodate home occupations in residential areas.

The office/retail component of the Gray's Crossing Specific Plan allows for live-work lofts to be constructed within portions of the Village Center.

By combining living and working space in a single unit, home-ownership is possible for many self-employed households. Live-work areas can also provide affordable rental housing and work space for artists, small businesses and start-up businesses. Under-utilized commercial buildings can be converted into live-work space, providing needed affordable housing and eliminating commute trips.

Land Use Policy 4.4

In the Development Code establish standards which preclude new "strip" commercial development. Site design for new commercial projects should provide pedestrian/bicycle access and have buildings properly proportioned to the pedestrian realm.

The Design Standards of the Gray's Crossing Specific Plan provide criteria to avoid strip commercial development. The Design Standards specify clustering, massing, landscaping techniques and architectural design features implemented in an overall building development to ensure that strip commercial development does not occur.

The circulation element within the Specific Plan provides for bicycle paths and pedestrian walkways connecting the office/retail, recreational components and residential land uses within the Plan.

A pedestrian and bike path (as shown on the Trails Plan) depict the following:

1. Pedestrian and bike lane that connects Prosser Lakeview Estates through the project to Downtown Truckee.
2. A pedestrian/mountain bike trail that follows the swale connecting properties to the east and west.
3. A proposed trail system on a series of existing on-site trails and dirt roadways that meander throughout open space areas within the Gray's Crossing property.

In summary, the pedestrian and bike lanes proposed for the project connect the adjacent subdivision with the Downtown Truckee area and the areas east and west of the project site. This connection is consistent with the comprehensive pedestrian and bike trailway system, as shown on Plate 5 of Truckee's General Plan and Figure 1.4 of the Circulation Plan for the Downtown Specific Plan.

Land Use Policy 4.5

Accommodate the establishment of satellite offices and local telecenters. Satellite offices and local telecenters provide places in or near residential areas for employees to use for telecommuting. Such facilities would provide computer and modem hookups and facilities for teleconferencing.

The Village Center District of Gray's Crossing permits telecommuting centers as a permitted use within this land use designation.

Land Use Policy 4.6

In the Development Code, establish standards to minimize incompatibility between adjacent land uses, including buffer yards, setbacks and Conditional Use Permit requirements.

The development standards within the Specific Plan provide design techniques to reduce impacts of incompatible land uses. Also, the land use designations within the Plan have been separated by open space or recreational land uses. Buffer strips between adjacent properties have been incorporated into the Plan. The combination of these elements will serve to minimize the potential incompatibility between adjacent land uses.

Land Use Goal 5

Preserve and enhance the distinctive look of Truckee and of each of its neighborhoods.

The Specific Plan's Design Guidelines specify design criteria for development of each component within the Plan. These design guidelines were developed in concert with the Truckee Design Guidelines and include design features, such as landscaping, site development and orientation requirements. These components ensure that the unique flavor of Truckee and its architecture will be maintained and enhanced.

Land Use Policy 5.1

Adopt and implement updated Design Standards as part of the Development Code. The updated Design Standards shall establish the following:

1. A street regulating plan which identifies existing and future right of way, sidewalk and bike lane locations, street widths, setbacks, edge treatments (landscaping, lighting, etc.) and proportion/height of adjacent structures.
2. A pedestrian/bicycle component which identifies the Town's pedestrian precincts, the network of sidewalks and bike lanes, materials for sidewalks and crosswalks and links to transit.
3. A design vocabulary which identifies vernacular architectural style recommendations, a list of acceptable materials and colors, building, site and parking design guidelines to prohibit large single building forms, mass parking areas and maintenance requirements.

The requirements for Specific Plans for the Downtown, PC-1, PC-2 and Tahoe Donner and the special policies for the Donner Lake Community Area also implement Land Use Goal 5.

Land Use Policy 5.2

The development code shall establish a maximum size limit for a single retail commercial use building of 40,000 square feet. Exceptions to this size requirement may only be considered through preparation and approval of a Specific Plan in accordance with California Government Code Section 65450-65457.

The Gray's Crossing Specific Plan has elaborate design standards. The design and site development standards of the Plan offer the following:

1. Specific roadway sections that accommodate bicycle, pedestrian walkways, landscaping and setback components.
2. Pedestrian and bicycle trail links throughout the Plan that can connect to adjacent properties.
3. A Complete definition of the architectural standards to ensure compatibility with recommended design components.

Land Use Policy 5.3

In the Development Code establish maximum building setbacks from the road frontage to require commercial and mixed-use buildings to be located closer to the street, where appropriate and require off-street parking areas to be located to the rear of commercial buildings, where feasible. Ultimate building locations must accommodate snow removal and snow storage and should maximize solar orientation.

Land Use Policy 5.4

Maintain and enhance Downtown as the heart of Truckee and as the Town's premier tourist destination through the following methods:

- Aggressively facilitate pedestrian oriented development Downtown by pursuing a wide range of mechanisms as identified in the policies for the Downtown Study Area on Page 54.
- The expenditure of capital improvement funds on projects that will enhance appropriate uses Downtown and facilitate new development implementing the Downtown Specific Plan will receive some priority.
- Allocation of staff resources to implement the Downtown Specific Plan will receive some priority.
- Require an independent economic analysis prior to approval for each of the Specific Plans. Use the analysis to understand the economic impacts of the proposed Planned Community Specific Plan on the Downtown and the community as a whole and to identify possible competition with significant new development planned downtown.

The design standards within each of the various zoning districts have specific criteria for site development including setbacks, road frontage and a mixture of specified uses.

The design features within the Specific Plan provide parking features to place parking facilities behind structures and break-up parking facilities with landscaping and other natural features to minimize large paved areas.

The General Plan allows for 175,000 square feet of office/retail use. The Gray's Crossing Specific Plan proposes only 47,900 square feet designed to service local residential and recreational needs. Limiting the square footage for office/retail and providing championship golf with lodging will attract tourists to the area that use the Downtown area for dining and shopping thus enhancing the existing Downtown and the "Heart of Truckee". The proposed office/retail space has been sized and designed so that it will provide neighborhood retail service and will not compete with existing retail service located within the Downtown core.

Land Use Policy 5.4 *(continued)*

Within the Planned Communities, delay construction of commercial development which competes with significant new development planned downtown until the above strategies are in place or significant additional development downtown is determined to be infeasible. Unless a longer time frame is identified in the Planned Community Specific Plans, such a delay shall not exceed two years from adoption of the General Plan or May 1, 1998, whichever comes first.

Land Use Goal 6

Regulate the size, quantity and location of signs to maintain and enhance the visual appearance of the Town.

The Specific Plan design guidelines provide specific sign criteria for all zoning components within the Specific Plan.

Land Use Policy 6.1

Prepare a comprehensive sign ordinance for the Town that seeks to eliminate existing billboards and establishes size and design requirements for all new signs, allowing an adequate time period for sign owners to amortize the value of their signs pursuant to an amortization schedule to be included in the sign ordinance.

The Specific Plan provides a strict design ordinance component and review process for approval of signs within the Plan area.

Land Use Goal 7

In all Residential and Residential Cluster land use categories, require clustering where appropriate to create efficient development patterns and to minimize environmental impacts and threats to public safety.

Clustering of residential development will allow flexibility of site design in responding to the natural features and resources of an individual site. Clustering means that structures will be located on a site so that larger areas are left undeveloped. The undeveloped areas may either be preserved in private or public open space or may be a portion of an individual lot, with deed restrictions prohibiting construction in that portion.

Land Use Policy 7.1

Residential development shall be clustered to avoid areas of significant natural resources, including wildlife habitat, migration corridors and visual resources.

Land Use Policy 7.2

The portions of parcels not developed with clustered residential uses shall be preserved as open space. Preservation and management options for open space include:

- Dedication to a homeowners association.
- Dedication to a public agency such as the Parks District or to a land trust or other non-profit agency.
- Use of building envelopes in conjunction with conservation easements or deed restrictions.

All residential development proposed within the Specific Plan is clustered into specified locations and surrounded by zoned open space. Within the Residential District for single-family homes, site development standards have been proposed which restrict development to 50% of the lot area. This allows additional open space areas of 50% of the land within that District.

Clustered residential areas will be located adjacent to existing residential areas. The riparian/wetland area bisecting the property from the southwest corner to the northeast is designated open space.

The Specific Plan proposes detailed site design criteria that limits types of fencing, minimizes impervious surface coverage and requires homeowner associations to administer and maintain these guidelines.

Gray's Crossing Specific Plan also provides opportunities for local park districts and/or land trusts to maintain lands designated and zoned open space within the development.

Planned Community 2 (PC-2)

The following policies will guide development of the Specific Plan for PC-2.

PC-2 Policy 1

Preserve existing natural features and wildlife habitat.

The Specific Plan has set aside natural areas to be zoned permanent open space.

PC-2 Policy 2

Preserve open space corridors connecting to adjacent open space lands to protect wildlife habitat.

The open space corridors throughout the Specific Plan connect to adjacent properties that are undeveloped.

PC-2 Policy 3

The planned community shall be sited oriented and massed to provide for maximum exposure to winter sun and protection from wind and temperature extremes.

The area for residential development and the Village Center have been oriented in a north-south alignment to take advantage of southern exposure available to the site. This orientation will provide winter sun to the buildings.

PC-2 Policy 4

Landscaping shall complement and be a component of the existing native ecosystem.

The Specific Plan provides detailed landscaping criterion to be incorporated into each design phase of the project.

PC-2 Policy 5

Maintain open space characteristics in the viewsheds along State Highway 89 and Interstate 80.

A large setback has been proposed along State Highway 89.

PC-2 Policy 6

The site shall be developed as a destination recreational community.

Each District (land use designation) provides opportunities for designated recreational land uses. These opportunities are listed in the permitted uses within each designation.

PC-2 Policy 7

Provide amenities such as golf course, riding stables, cross-country ski area, hiking/biking trails, recreational center and hotel.

The Specific Plan proposes a golf course that can be utilized during the winter months for cross-country skiing and snow-shoeing and during the summer months for golfing, walking or jogging. Also, the open space corridor within the Plan provides opportunities for trails and a variety of other passive recreational uses. The recreational land use provides lodging opportunities in conjunction with the golf course and clubhouse facility.

PC-2 Policy 8

Residential uses shall be clustered. Apartment units shall be interspersed with office and light commercial uses in some areas. Affordable housing shall be provided in addition to market rate housing. The number of affordable units shall be equal to twenty percent of the number of market rate units.

Residential units have been clustered in order to increase the amount of open space. In addition, affordable housing units will be dispersed throughout these clustered areas.

PC-2 Policy 9

Retail shopping areas shall be designed at a pedestrian scale, incorporating pathways, courtyards and other activity nodes and a mix of shopping, offices, residences and services. Large single building forms and large masses of pavement are not considered consistent with this policy.

The design guidelines within the Specific Plan set forth specific design criteria to accomplish pedestrian scale development with pathways, courtyards and activity nodes.

PC-2 Policy 10

Architecture shall be consistent with the Town of Truckee Design Guidelines.

The architectural design guidelines were developed in concert with the Truckee Design Guidelines.

PC-2 Policy 11

Parking areas shall be screened. Parking shall be provided in scattered small lots or located to the rear of buildings.

The design guidelines specify screened parking areas and scattered small parking lots.

PC-2 Policy 12

Incorporate pedestrian, equestrian, bicycle and ski trails into project design to provide access through the entire project area, connecting onsite and offsite shopping and service areas with residential and recreational areas. Trails shall be open to the public.

The Specific Plan allows and sets aside areas for pedestrian, bicycle and ski trails. Depending on the actual zoning of these particular areas and coordination with other districts, these areas may be available for public use.

PC-2 Policy 13

Provide pedestrian/bicycle/cross-country ski links to the downtown area and connecting with trails on adjacent properties.

The Plan provides for open space connections to adjacent properties which offer a linkage to the Downtown area.

PC-2 Policy 14

The PC-2 Specific Plan shall provide a reasonable balance of residential, retail, office and recreational land uses in each phase.

The proposed Phasing Plan outlines the various uses in each phase. The golf course will be constructed in the first Phase. This will develop the recreation component that creates the demand for residential.

PC-2 Policy 15

Maximum development standards contained in Table 1.4 will be reevaluated during future general plan updates to determine if there is a need to allocate additional development potential to PC-2.

The triangular property between Interstate 80 and the new State Highway 267 Bypass will remain undeveloped and can be reevaluated in future General Plan updates.

Circulation

Circulation Goal 1

Provide for the long-range development of the Town's roadway system consistent with adopted land use patterns, ensure the safe and efficient movement of people and goods and implement funding strategies for construction, improvement and maintenance of existing and new roadways.

Providing neighborhood retail services on the north side of Interstate 80 provides alternatives for residents north of Interstate 80 to meet some of their retail needs in areas other than the west end of Town. This "interceptor concept" allows residents north of Interstate 80 to avoid the congestion that occurs on Donner Pass Road during peak traffic hours.

Circulation Policy 1.2

Ensure that existing and new streets and roads are dedicated, widened and constructed according to roadway design and access standards adopted by the Town. Permit exceptions only upon determination by the Public Works Department that the exceptions do not compromise public safety and that adequate public access and circulation are preserved.

The Specific Plan incorporates road standards for roadway sections, right-of-ways and improvements to all roadways within the development areas.

Circulation Policy 1.3

Prepare and implement road, sidewalk and bikeway standards, which recognize Truckee's climatic conditions in order to reduce long-term maintenance costs of the road system.

The roadway section details of the Specific Plan allow for pedestrian walkways and bicycle paths.

Circulation Policy 1.4

Where feasible, ensure that traffic control devices, lighting and other traffic safety and operational improvements are installed when necessary for the safe and efficient movement of all types of traffic and acceptable levels of service.

The Gray's Crossing Specific Plan has specified highway and roadway encroachments to each of the development areas. These specified areas provide the potential for controlled access subject to the traffic warrants at the time of development.

Circulation Policy 1.7

Maintain Donner Pass Road at a three-lane cross-section (two lanes of traffic with a left-turn lane). New projects which could add significant traffic to Donner Pass Road must demonstrate that cumulative traffic impacts will not result in the need to widen Donner Pass Road.

With the implementation of the Tahoe-Donner Roadway Connector, the potential to maintain Donner Pass Road as a three-lane road section is assured. With cumulative growth in the area and without the Tahoe-Donner Bypass, continued congestion of that roadway would necessitate widening Donner Pass Road. Implementation of the Tahoe-Donner Roadway Connector and providing some neighborhood retail services on the north side of Interstate 80 can help to avoid the necessity to widen Donner Pass Road and help to reduce current and projected traffic congestion.

Circulation Policy 1.8

Require the preparation of traffic impact analyses to identify impacts and mitigation measures for projects, which may result in significant traffic impacts. Level of Service shall be computed according to the planning methodology as documented in Circular 212, Interim Materials on Highway Capacity, Transportation Research Board, January 1980. Cumulative impacts shall be modeled assuming full build-out of the General Plan.

A traffic analysis based on the projected growth of the Specific Plan area and the cumulative growth based on Truckee's General Plan will determine the actual roadway improvements and requirements for signalization from encroachments on State Highway 89. Based on the roadway capacity of Alder Drive and Prosser Dam Road and the proposed levels of development, it is not anticipated that any major improvements other than bicycle paths and pedestrian walkways will be needed in these areas.

Circulation Policy 1.12

Construct a third access road out of Tahoe Donner, which connects to State Highway 89 North and the Downtown area.

The Gray's Crossing Specific Plan provides some right-of-way through the school site that can help distribute the traffic using the proposed Tahoe-Donner Roadway Connector.

Circulation Policy 1.17

Reevaluate parking requirements in the Development Code to ensure that excessive parking is not required and to address options for shared parking and other parking lot alternatives.

The Gray's Crossing Specific Plan has specific parking and landscaping requirements based on the uses within the Plan.

Circulation Policy 1.18

Consider the visual impacts of off-street parking lots during project review and implement design standards to locate parking to the rear of buildings or provide substantial screening of parking areas.

The design and landscape standards of the Gray's Crossing Specific Plan specify criteria for parking lot development to offset adverse visual impacts resulting from such uses.

The setbacks, incorporation of existing trees in the parking lot design and the building locations themselves provide on-site design features that minimize visual impacts resulting from parking areas.

Circulation Goal 3

Promote a safe and efficient transit system, including both rail and bus, to reduce congestion, improve the environment and provide viable non-automotive means of transportation in and through Truckee.

The Village Center and Residential Districts within the Specific Plan will provide transit stops and turnouts should the local authority wish to extend transit services to this area. Also, the homeowners association within this development can participate in the local Transportation Management Association (TMA) to stimulate carpooling, tele-communications and centralized transit stops.

Circulation Policy 3.1

Work with transit providers to plan and implement additional transit services within and to the Town that are timely, cost-effective and responsive to growth patterns and existing and future transit demand.

By joining the local TMA, coordination with local transportation providers can be achieved.

Circulation Policy 3.2

Consider the need for future transit right-of-way in reviewing and approving plans for development and roadway construction or improvements. Incorporate features to encourage transit and reserve right-of-way for future transit access in plans for new growth areas. Transit rights-of-way may either be exclusive or shared with other vehicles.

Roadways within the Specific Plan will have sufficient right-of-way to accommodate transit stops within the development.

Circulation Policy 3.7

Require new development to provide sheltered public transit stops with turnouts where appropriate. Consider development of turnouts in existing developed areas when roadway improvements are made or as deemed necessary for traffic flow and public safety.

Residential and Village Center Districts will provide transit stops at specified locations.

Circulation Goal 4

Maximize the efficient use of transportation facilities so as to:

- Reduce travel demand on the Town's roadway system;
- Reduce the amount of investment required in new or expanded facilities needed to accommodate increased demand on the Town's roadway system;
- Reduce the quantity of emission of pollutants from automobiles; and
- Increase the energy efficiency of the transportation system.

The Gray's Crossing Specific Plan provides a unique opportunity for needed neighborhood retail, recreational and residential amenities without impacting local roadway systems. Providing these amenities north of Interstate 80 with primary access from Interstate 80 and State Highway 267 reduces the travel demand on local roadways and the amount of investment needed to maintain local roadways.

Circulation Policy 4.1

Promote the use of transportation control measures (TCMs) that divert automobile trips to transit, walking and bicycling, through planning and provision of appropriate facilities and incentives. TCMs shall include the following:

- Passenger rail,
- Trip reduction programs,
- Telecommunications, support for telecommuting,
- Traffic flow improvements,
- Park-and-ride lots,
- Ride-share programs,
- Bicycling programs,
- Short-range transit and
- Alternate work schedules.

By participating in the local transportation management association, coordinating efforts to promote the use of transportation alternative measures by increasing pedestrian walkways, bicycle paths and car pooling opportunities are available. Promoting telecommunications and telecommunication centers within the Village Center District further assists in promoting what is currently classified by the California Division of Highways as "smart communities."

Circulation Goal 5

Provide a safe, comprehensive and integrated system of facilities for non-motorized transportation to meet the needs of commuters and recreational uses and to provide an alternative to auto transportation.

The Gray's Crossing Specific Plan provides a roadway network that incorporates bicycle paths and pedestrian walkways. The open space areas provide the opportunity for a trail system that has the potential to network with adjacent properties.

Circulation Policy 5.1

Work with the Truckee Donner Recreation and Park District to prepare and implement a Bicycle and Pedestrian Trail Master Plan to establish a comprehensive and safe system of bicycle routes and pedestrian trails for short-range commuting, shopping trips and for recreation use.

Public land use has been provided and made available to the Truckee Donner Recreation and Park District. By locating those lands adjacent to open space and the existing roadway, the Plan provides a direct opportunity for Truckee Donner Recreation and Park District to implement a bicycle and pedestrian master plan that serves this property as well as adjacent properties.

The Pedestrian and Bike Trail System Plan shows how the Gray's Crossing project can and will link surrounding properties to the Downtown Truckee area as shown in Figure 1.4 of the Downtown Specific Plan and the Trail System Plan shown on Plate 5 of the Truckee General Plan.

Circulation Policy 5.2

New bikeways shall be linked with other bikeways and paths to provide safe continuous routes.

PC-2 is the hub that will connect surrounding properties, including Tahoe Donner, Prosser Lakeview Estates, Prosser Heights and surrounding subdivisions with a transportation system that includes pedestrian, bike and public transit options. In addition, trolley stops are proposed to serve the Village Center District, adjacent Coachland Mobile Home Park, school site and the lodging facility.

Circulation Policy 5.3

Encourage the provision of bicycle routes along State highways, especially a bicycle/pedestrian facility along State Highway 89 under the Union Pacific tracks.

The Gray's Crossing Specific Plan allows new bicycle paths and pedestrian walkways to be connected to existing roadways, i.e., Prosser Dam Road, State Highway 89 and Alder Drive.

Circulation Policy 5.4

Pursue all available sources of funding for the development and improvement of trails for non-motorized transportation (bikeways and pedestrian and equestrian trails).

The Gray's Crossing Specific Plan will provide a trail network, bicycle paths and pedestrian walkways throughout the development.

Circulation Policy 5.6

Establish pedestrian and bicycle access standards and require developers to finance and install pedestrian walkways, equestrian trails and multi-use trails on new development, as appropriate and necessary to address circulation needs.

The Gray's Crossing Specific Plan will provide bicycle paths, pedestrian walkways, trails and multi-use trails.

Circulation Goal 8

Promote the efficient movement of goods and people within new growth areas and between growth areas and other major destinations in the Town and region.

New growth areas include the new Planned Communities and the Residential Cluster areas.

Circulation Policy 8.1

Require that land use patterns and transportation systems in designated new growth areas be designed to provide residents and employees with the opportunity to accomplish many of their trips within the new growth areas by walking, bicycling and using transit.

The road system within Gray's Crossing includes bike trails, walking corridors and connection trails throughout the project. This system increases and encourages the opportunities for pedestrians and bicycle access to various components within the project.

Circulation Policy 8.2

Require that transportation systems and improvements planned and constructed in designated new growth areas provide links to transportation systems outside the new growth area and address impacts on transportation facilities outside the new growth area.

Circulation within Gray's Crossing allows for connection points to subdivisions in the north and west of the subject property. Connection to the Village Center and various recreational components of the project allow for access from these various areas without having to travel on State Highway 89. In addition, the proposed public land use in the southwestern portion of the property provides an alignment for connection to the Tahoe-Donner Roadway Connector, a major transportation route shown on Truckee's General Plan.

Conservation and Open Space

Conservation and Open Space Goal 1

Protect areas of significant wildlife habitat, fisheries and biological resources.

Conservation and Open Space Policy 1.3

Provide for the integrity and continuity of wildlife habitat and support the permanent protection and restoration of sensitive wildlife habitat and wildlife movement corridors through a variety of tools, including preservation in open space and the use of planning fee waivers for projects that provide for permanent protection of such areas.

Sensitive wildlife habitat includes the following:

- Meadows, wetlands and riparian corridors;
- Deer migration/wildlife movement corridors;
- Deer fawning areas;
- Habitat for State and Federally listed plant and animal species; and
- Large blocks of undeveloped forest.

The Gray's Crossing Specific Plan provides approximately 417 acres (55%) of unobstructed open space land. Sensitive areas, such as wetlands and open meadows, have been preserved through this open space zoning. Recreational uses that are allowed in the open space areas have been delineated and restricted to areas not considered to be sensitive wildlife habitat or important migratory movement areas. The 52-acre parcel to be acquired on the east of the property line has been surveyed by the biologist to determine if there are any sensitive habitats. The conclusion of these studies can be found in the "Habitat Management Conservation Plan" prepared by EcoSynthesis for the environmental documentation that was part of the Truckee Sierra Resort and Spa Development permit issued by the Town of Truckee.

Conservation and Open Space Goal 2

Protect and conserve forest and rangeland resources for their production of wood products, livestock uses and recreational, aesthetic and biological values.

Conservation and Open Space Policy 2.1

In the Development Code establish standards for buffering residential uses from adjacent forest resources to minimize conflicts resulting from timber harvesting.

In addition to the large area of land devoted to permanent open space, the Residential District for single-family units has a lot coverage restriction that cannot exceed 50% of the total lot area. The remaining land area will be left undisturbed.

Conservation and Open Space Policy 2.4

Through the review of discretionary projects the Town shall require the protection, planting and regeneration of native plant species in undisturbed portions of the project site.

The landscaping component within the Specific Plan provides for a landscaping theme in each of the plan use areas. The Plant Palette in the Specific Plan depicts the various types of indigenous plants likely to survive in the area. Also, each design standard within the Specific Plan encourages and requires the retention of existing trees to be incorporated into the landscaping component of each design.

Conservation and Open Space Goal 4

Protect and preserve important views and visual resources. Preserve significant scenic features in open space where appropriate.

Conservation and Open Space Policy 4.2

Locate new development outside of scenic vistas and prominent slope exposures and ridgelines identified on the Open Space, Natural/Scenic Resources and Trails Map.

The Development Area Map within the Specific Plan clearly depicts the open space areas that reserve scenic vistas and restrict development on dominant slope exposures and ridgelines.

Conservation and Open Space Policy 4.3

In the Development Code designate Scenic Highway Corridors and establish a plan for preservation of views from Interstate 80, State Highway 89 North and other roadways to be designated. Implement the Scenic Corridor combining district to regulate the appearance of projects located within the viewsheds of existing and future scenic corridors.

The Land Use Plan of the Gray's Crossing Project incorporates an open space scenic corridor along State Highway 89 North. Large setbacks and design restrictions within each of the various zones ensures maintenance of scenic corridors along State Highway 89.

The Gray's Crossing project provides setbacks, retention of existing trees, design features, design standards and specified locations of development that all combine to maintain and preserve the views from Interstate 80 and State Highway 89 North. In addition, specific design standards maintain the existing scenic quality of Prosser Dam Road, Alder Road and areas adjacent to the Coachland Mobile Home Park.

Conservation and Open Space Policy 4.4

In the Development Code establish standards for new development to protect visible hillsides and ridges. These standards will address screening, design and setbacks from the tops of ridges.

The Specific Plan for the Gray's Crossing project provides design guidelines and restrictions that address screening, design, setbacks and evaluation of each site taking into account natural constraints, existing topography and tree protection.

Conservation and Open Space Policy 4.5

In areas where clustering is used, apply Open Space zoning to protect scenic vistas, prominent slope exposures and ridgelines as identified on the Open Space, Natural/Scenic Resources and Trails Map.

The land use design of the Gray's Crossing Project incorporates the clustering concept. The development maps in this Specific Plan also depict areas devoted to open space. Within the use areas, design restrictions within each of the land use categories are applied to further assist in preservation of existing ambience of the area.

Conservation and Open Space Goal 5

Preserve the scenic beauty of Truckee through the Development Review Process.

Conservation and Open Space Policy 5.1

In the Development Code establish standards for outdoor lighting to reduce light pollution.

The Design Standards within the Specific Plan state the type and requirements for lighting within each component of uses within the development area.

Conservation and Open Space Policy 5.2

Require that all structures at the existing Interstate 80/State Highway 89 North interchange be substantially screened from sight from the highway. Require preservation of existing vegetation to screen new developments visible from Interstate 80 and where necessary, require additional landscaping to screen buildings and other facilities. In implementing this policy, recognize the need to provide for visibility of freeway oriented commercial uses.

The Aerial Photo delineates the vegetation proposed to remain that would provide substantial screening from developed areas and roadway right-of-ways. The Design Standards also specify types of landscaping, landscaping concepts and design features to further provide a pleasing atmosphere from passing motorists through the state and federal highways bisecting the property. The setbacks, photo details, photo simulation and design criteria submitted demonstrates how preservation of existing vegetation will be maintained along Interstate 80 and State Highway 89 North.

Conservation and Open Space Policy 5.4

Require undergrounding of all new utilities wherever practical. Encourage other agencies and entities to underground their facilities.

As specified in the Gray's Crossing Specific Plan the utilities installed must be in compliance with provisions of the Truckee Donner Public Utility District. Underground utilities will be applied, where possible, to all land use categories within the Specific Plan.

Conservation and Open Space Policy 6.1

Minimize the loss of groundwater recharge areas from paving. In the Development Code establish coverage limitations and encourage the use of permeable paving materials.

Each land use category within the Specific Plan has lot coverage restrictions that limit the amount of impervious surface materials.

Conservation and Open Space Policy 6.2

Protect surface and groundwater from contamination through runoff by implementing the Regional Water Quality Control Board, Lahontan Region's, Best Management Practices.

Land use standards within Recreation District Development Standard of the Specific Plan clearly define the techniques to be incorporated in managing surface runoff. These standards are in conformance with the Best Management Practices of the Tahoe Region and supported by the California Regional Water Quality Control Board, Lahontan Region.

Conservation and Open Space Goal 7

Conserve and protect the Town's Soil Resources by minimizing soil erosion.

Conservation and Open Space Policy 7.1

Discretionary development shall be clustered away from slopes in excess of 30%. Discretionary development on all slopes in excess of 20% shall have a site specific review of soil type, vegetation, drainage, slope and building placement to determine proper site design.

The elements of the Specific Plan limit the development on excessive slopes in three different ways:

- Open Space Zoning;
- Site Development Restrictions within each land use category; and
- Design standards within the Specific Plan.

Conservation and Open Space Policy 7.2

On discretionary projects that require earthwork and grading, including cuts and fills for roads, developers shall be required to minimize erosion and sedimentation by conforming with the natural contours, maximizing retention of natural vegetation, and implementing Best Management Practices.

The Natural Resources Management Plan (NRMP) and the Landscape Design Program and Design Guidelines of the Gray's Crossing Specific Plan provide detailed criteria to direct grading, which adheres to natural contours and Best Management Practices Standards.

Conservation and Open Space Goal 8

Protect and increase opportunities for outdoor recreation.

Conservation and Open Space Policy 8.1

Establish a town-wide multi-use public trail system through the development and adoption of a Trails and Bikeways Master Plan. Include in the Master Plan a Donner Creek parkway to link the Donner Memorial State Park to the Truckee River. In the interim, prior to adoption of the Trails Master Plan, require preservation of trail corridors shown on the Conceptual Trails Plan through review of new discretionary development on parcels containing these trails. Coordinate with adjacent jurisdictions to identify trail linkages outside of Town limits.

The open space corridors throughout the Gray's Crossing Specific Plan provides for trail dedication in conformance with the Conceptual Trail Plan of Truckee's General Plan.

The Conceptual Trails Plan is shown on Plate 5.

Conservation and Open Space Policy 8.2

Encourage the development of destination resorts incorporating outdoor recreation facilities. Consider incentives for recreational facilities open to public use.

The recreational component of the Specific Plan is designed to accommodate a destination recreation community. Gray's Crossing is one of the few areas that is most likely to accommodate a destination recreation community due to its large acreage and ability for recreational amenities, such as golf, hiking trails, bicycle lanes, snow-shoeing and cross-country ski courses.

Conservation and Open Space Policy 8.5

Cooperate with the Truckee Donner Recreation and Park District to improve and maximize the use of existing recreational facilities and to plan for new facilities to accommodate residents and visitors. Work with the District to update and implement their Parks Master Plan.

Portions of the property are being offered to the School District and Truckee Donner Recreation and Park District.

Conservation and Open Space Goal 9

Protect cultural and historic resources and accommodate public access as appropriate.

Conservation and Open Space Policy 9.1

Require evaluation of impacts to cultural resources for projects, which involve substantial site disturbance.

Past land use permits on this property have included an archaeological reconnaissance and excavation of important cultural resources. In accordance with past environmental documentation cultural resources have been properly investigated, identified and preserved in various manners. Prior to development within each individual component, it is recommended that a resource evaluation be conducted to more thoroughly evaluate specific areas where site disturbance is proposed.

Conservation and Open Space Goal 12

Encourage project design that protects air quality and minimizes direct and indirect emissions of air contaminants.

Conservation and Open Space Policy 12.1

Reduce automobile dependency by permitting mixed land use patterns which locate services such as banks, child care facilities, schools, neighborhood shopping centers and restaurants in close proximity to employment centers and residential neighborhoods.

The Gray's Crossing Village Center District offers a neighborhood retail component with a variety of land uses that can reduce the automobile dependence by providing some alternative shopping north of Interstate 80 for local residents. The Village Center is located so that pedestrian and bike paths can connect to the surrounding residential development within the project as well as other residential developments that surround the property.

Conservation and Open Space Policy 12.2

All discretionary development applications shall be reviewed to determine the need for pedestrian/bike paths connecting to adjacent development and services in order to provide alternatives to automobile transportation.

Each land use component within the Specific Plan provides for bike lanes and/or pedestrian hiking paths to connect to various land uses within the project plan. These trails and routes also connect to adjacent properties to facilitate pedestrian and bicycle use throughout the area.

Safety

Safety Goal 1

Establish standards to reduce threat of hazards to life and property and direct new development away from natural hazard areas.

Safety Policy 1.1

In Residential and Residential Cluster designations, new residential development shall be clustered to avoid areas of hazard, including high fire hazard, steep slopes, areas of unstable soils, avalanche or flooding.

The Gray's Crossing Specific Plan proposes to utilize the clustered concept. The areas devoted to development avoid hazard areas with high fire danger and steep slopes.

Safety Policy 2.6

Prevent increases in downstream flooding potential through establishment of guidelines, which regulate the rate of offsite run-off for projects, which require Town review and approval.

The Program Section of the Specific Plan specifies drainage standards and onsite retention that minimizes the impact of downstream flooding as a result of development on this site.

Safety Policy 2.7

Require that adequate emergency water flow, emergency vehicle access and evacuation routes be available with approval of any new development.

The circulation pattern within the Gray's Crossing Specific Plan provides connection routes for existing development as well as through circulation to the proposed development. Providing access connections on Prosser Dam Road provides alternative access for residents in Prosser Lakeview Estates. The Tahoe-Donner Roadway Connector will provide emergency and access circulation for the residents of Tahoe Donner, Alder Drive and Gray's Crossing.

Safety Policy 2.8

Cooperate with the Fire District to implement fire safety ordinances to attain and maintain "defensible space" to minimize wild land fire hazards. Defensible space may include revegetation with less flammable species, such as fire resistant native and adapted species and the use of mulch to prevent erosion on bare soil.

General Development Standards for each land use District in the Specific Plan specify development restrictions and design standards that maintain defensible space to minimize wild land fire hazards.

Noise

Noise Goal 1

Provide appropriate exterior and interior noise environments for land uses to protect citizens from excessive noise.

Noise Policy 1.1

Require all new development projects to comply with the standards identified in the Land Use Compatibility with Noise Table in this element and the California Noise Insulation Standards for interior noise levels.

A combination of setbacks, design standards and location of development all ensure that noise sensitive land uses will be compatible with Noise Table 6.2 of the Noise Element of Truckee's General Plan.

Noise Policy 3.5

As an alternative to construction of sound walls, require developers to use site design techniques, building setbacks and alternative architectural layouts as a means of meeting noise reduction requirements.

The Gray's Crossing Specific Plan utilizes setbacks, architectural design standards and site development standards throughout the various land use designations of the Plan to ensure noise sensitive land uses are not placed in locations vulnerable to noise impacts from surrounding roadways.

Housing

Housing Goal 1

Ensure an adequate supply of housing to meet the housing needs of all segments of the community.

Housing Policy 1.1

Provide adequate residential sites for the production of new residential units to meet the needs of existing and future residents.

Truckee's General Plan analyzed the projected population and housing demands to serve that population. To accomplish the goal of providing housing opportunities throughout Truckee, three planned communities were established; PC-2 (Gray's Crossing Specific Plan) is one of these areas. The housing opportunities provided in PC-2 require a variety of housing options, such as single-family, cottage, attached, affordable and employee residential units to accommodate the housing demands for the Truckee area. Implementation of the Gray's Crossing Specific Plan will comply with the goals of the Housing Element within Truckee's General Plan.

Housing Policy 1.2

Provide a sufficient amount of higher density residential land, distributed throughout the Town, to accommodate the Town's share of the regional housing need for affordable housing.

The Gray's Crossing Specific Plan has allocated approximately 12.7 acres for attached and high-density residential land uses. In addition, some residential uses are permitted within the Village Center District of the Gray's Crossing Specific Plan.

The Land Use Diagram implements this policy.

Housing Program 1.2.1

Require development of assisted units on scattered sites to avoid over concentration.

The General Plan Land Use Policy applicable to PC-2 allows for a maximum of 100 units within the planned area to be in the affordable housing range. Also, the various land use categories within the Plan allow for a variety of housing opportunities, such as assisted care units and units to accommodate the housing needs of a tourist oriented economy.

Housing Program 1.2.3

Permit secondary units by right in single-family residential areas where it is determined that adequate infrastructure exists to serve the second unit.

The Residential District within the Gray's Crossing Specific Plan allows for secondary units on the single-family lots, such as granny flats, and allows second unit apartments within existing residential structures.

Housing Program 3.1.9

As part of the revision of the Zoning Ordinance, allow shared parking in commercial and residential mixed use projects, where it can be demonstrated that the uses do not have competing peak parking demands.

The Village Center District within the Gray's Crossing Specific Plan allows for mixed residential units within office/retail areas.

Housing Goal 4

Balance the need and provision of housing in the community with its impacts on the environment and needed public facilities and services.

Housing Policy 4.1

Encourage residential design that promotes energy efficiency.

The design standards accompanying the Residential (single-family and cottage) and Attached Residential Districts of the Gray's Crossing Specific Plan provide for site development and solar orientation to ensure maximum energy efficiency within these areas.

Housing Policy 4.2

Encourage residential development design that clusters units to reduce infrastructure costs and other development costs, as well as to preserve and enhance important environmental resources and to maintain important areas as open space.

The Gray's Crossing Specific Plan incorporates the clustered concept. Clustering allows for efficiency in services and reduces the cost of providing those services to the individual areas within the development.

Housing Program 4.2.1

Adopt standards for residential cluster areas and mechanisms for the long term protection and maintenance of the open space areas within their residential cluster areas.

A variety of mechanisms can be employed to ensure the long-term protection of open space within the Gray's Crossing Specific Plan. Zoning of the land, granting of conservation easements and limitations on site development within each individual lot combine to ensure long-term preservation of open space land.

Policies relating to residential cluster uses are found in the Land Use, Conservation and Safety Elements.

Housing Program 5.1.4

Any projects creating 100 or more jobs will be subject to a Development Agreement with the Town wherein the Town may ask for consideration to address the impact of the jobs to be created on the Town's affordable housing supply.

Affordable housing units will be distributed throughout the project in the Residential and Attached Residential Districts, and employee housing will be offered in the Village Center.